

BISHOP AIRPORT

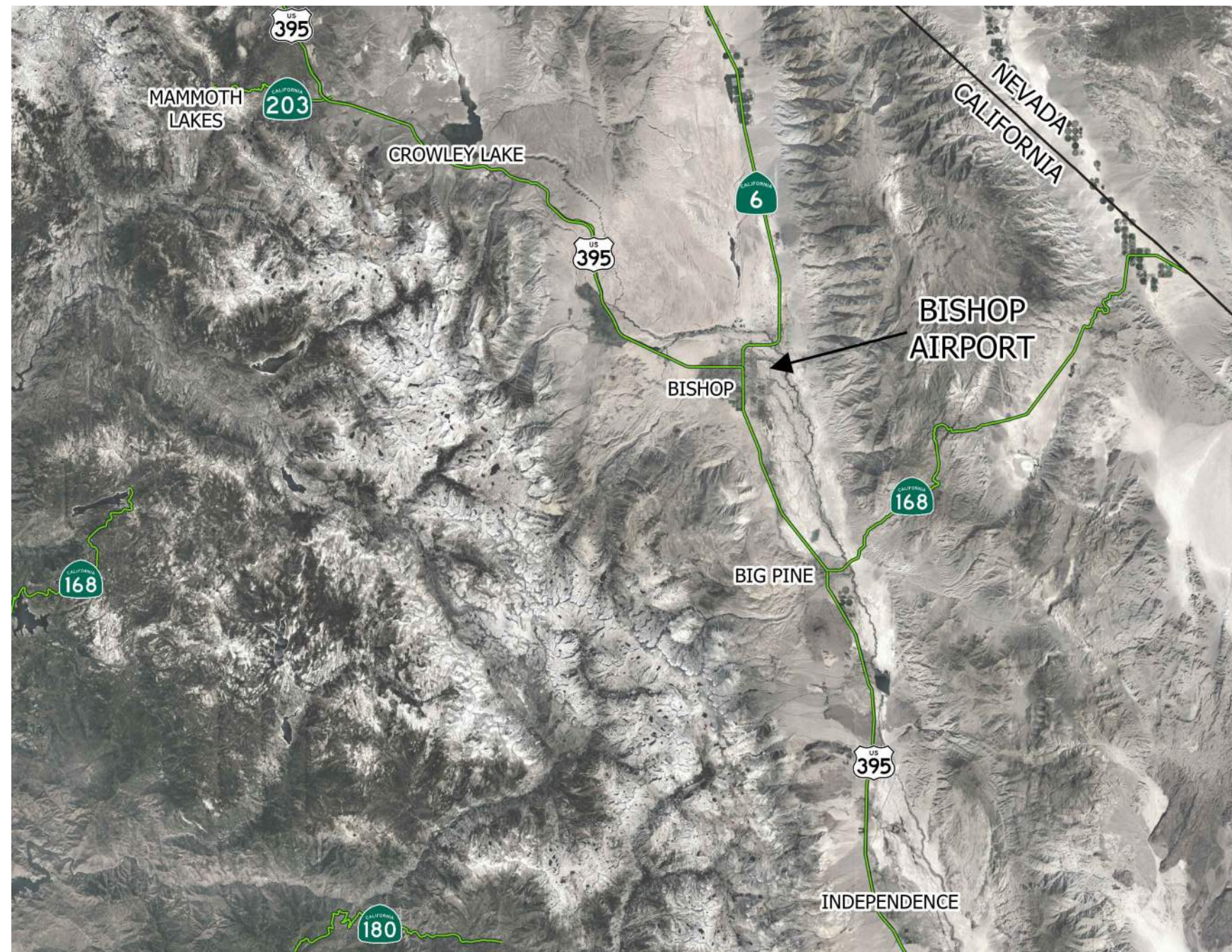
A COUNTY OF INYO AVIATION FACILITY

BISHOP, CALIFORNIA

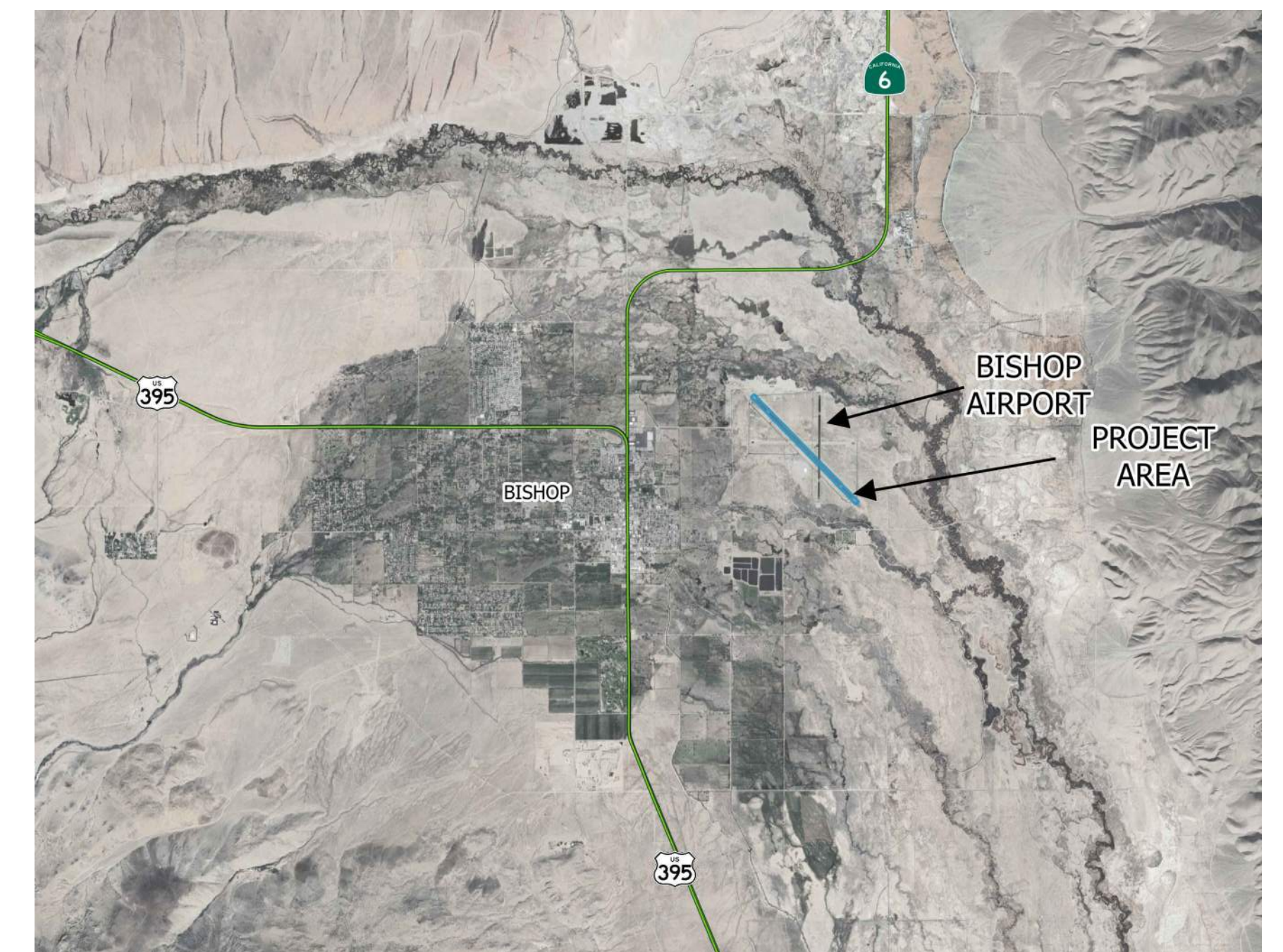
RUNWAY 12-30 SURFACE TREATMENT

INYO COUNTY PROJECT NO. 630100
AIP PROJECT NO. 3-06-0024-XXX-2024
JULY 2024

PLAN SET ISSUED FOR BIDDING PURPOSES ONLY



REGIONAL MAP
NO SCALE



VICINITY MAP
NO SCALE

COUNTY OFFICIALS

MICHAEL ERRANTE DIRECTOR OF PUBLIC WORKS
ASHLEY HELMS DEPUTY DIRECTOR OF PUBLIC WORKS—AIRPORT
STEVE LOVEN AIRPORT OPERATIONS SUPERVISOR

GENERAL PROJECT DESCRIPTION

PAVEMENT MAINTENANCE EFFORTS ON RUNWAY 12–30 AND CONNECTING TAXIWAYS, INCLUDING CRACK FILL AND SEALING, APPLICATION OF EMULSIFIED ASPHALT SEAL COAT, GROOVING ASPHALT SURFACE, AND APPLICATION OF NEW MARKINGS.

GENERAL CONSTRUCTION ELEMENTS

- AIRFIELD SAFETY AND SECURITY.
- CONSTRUCTION SITE ENVIRONMENTAL COMPLIANCE.
- PERFORM CRACK FILL AND SEAL, INCLUDING ROUTING OF EXISTING CRACKS AND PAVEMENT COLD JOINTS.
- OBLITERATE SOME PAVEMENT MARKINGS TO 50%.
- GROOVE EXISTING ASPHALT SURFACE.
- APPLY AN EMULSIFIED ASPHALT SEAL COAT TO THE RUNWAY, AND CONNECTING TAXIWAYS UP TO THE HOLD POSITION MARKINGS, USING A HAND SHIELD TO PREVENT SEAL COAT APPLICATION ON PAVEMENT MARKINGS INTENDED TO REMAIN.
- APPLY NEW PAVEMENT MARKINGS.

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10	MARKING PLAN	RUNWAY 12–30 STA: 42+00 TO STA. 76+94.81
11	DETAILS	SURFACE TREATMENT
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APPROVED BY Michael Errante DATE 7/2/24
MICHAEL J. ERRANTE, DIRECTOR
INYO COUNTY PUBLIC WORKS



SUBMITTED John A. Smith DATE 7/2/24
JOHN A. SMITH RCE 46852
PROJECT ENGINEER



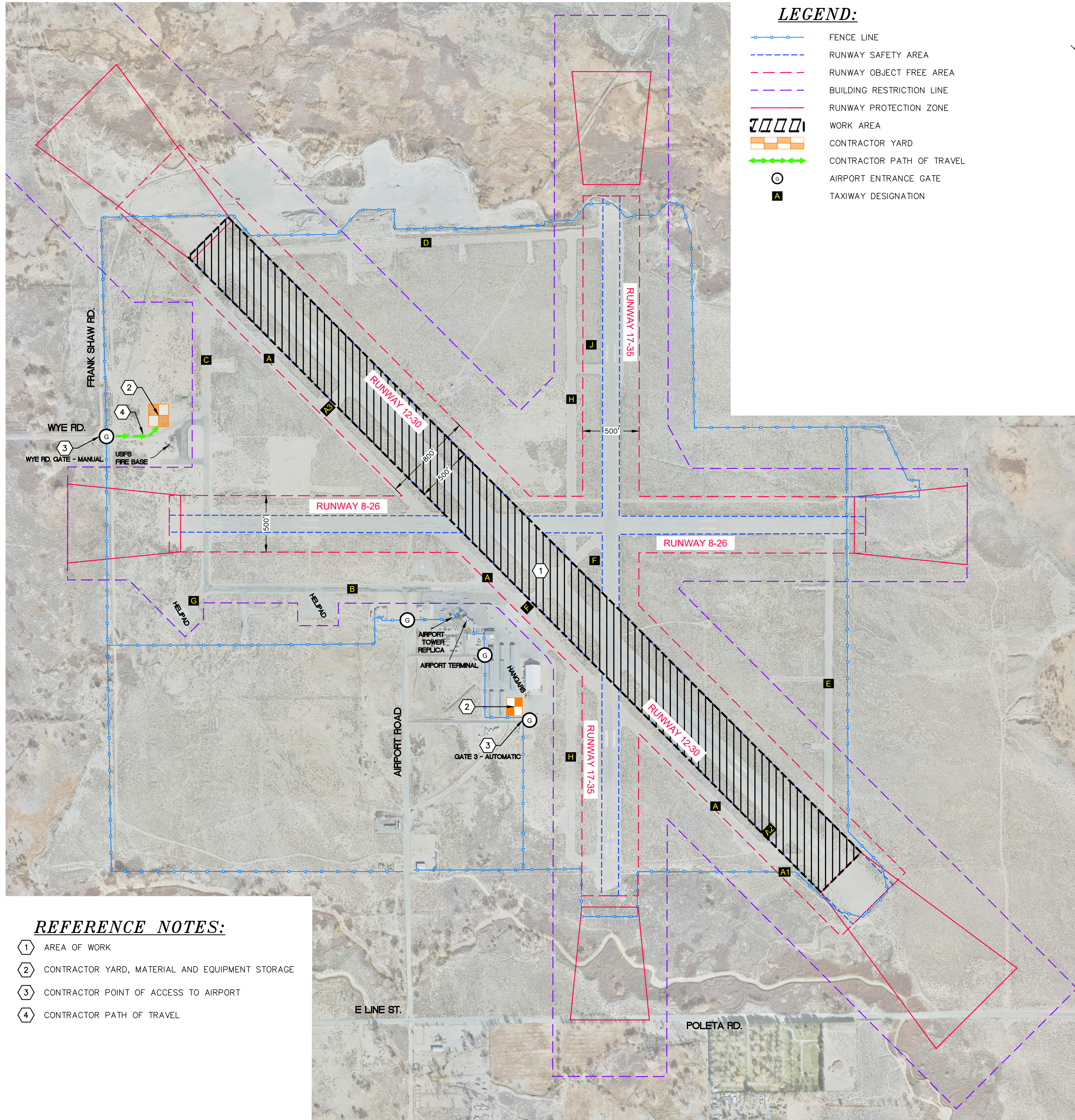
REV.	DESCRIPTION	DATE	APP.
1			

UNDERGROUND SERVICE ALERT
811 DIAL 811
TWO WORKING DAYS
BEFORE YOU DIG

01
SHEET

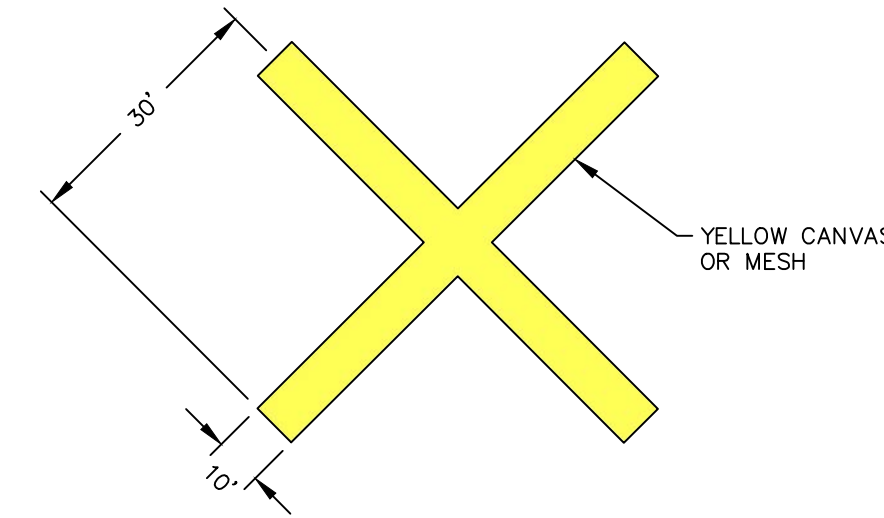
TARTAGLIA ENGINEERING

24-31
DRAWING NO.



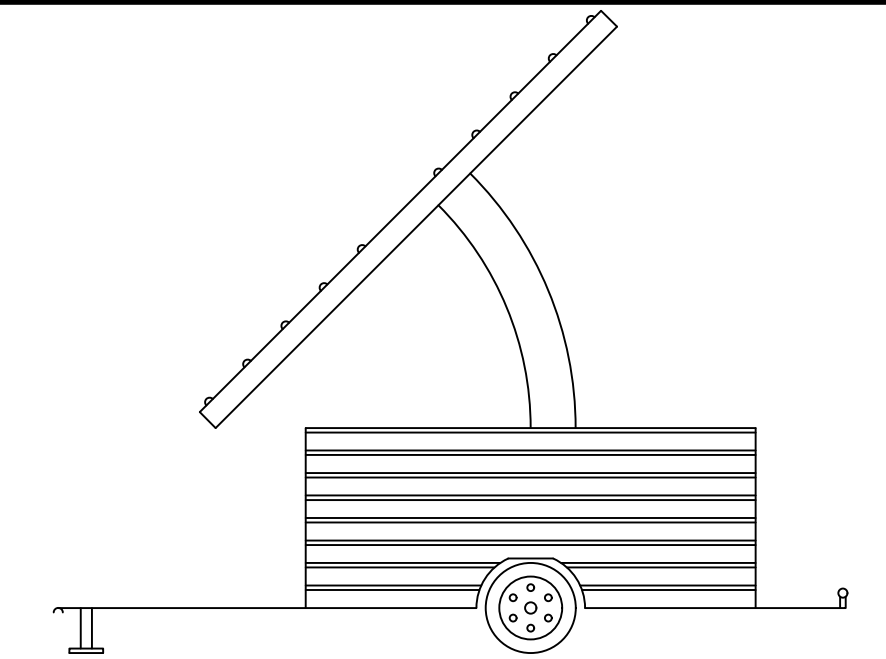
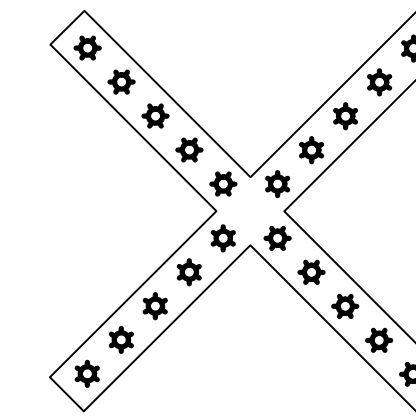
LEGEND:

- FENCE LINE
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- BUILDING RESTRICTION LINE
- RUNWAY PROTECTION ZONE
- WORK AREA
- CONTRACTOR YARD
- CONTRACTOR PATH OF TRAVEL
- AIRPORT ENTRANCE GATE
- TAXIWAY DESIGNATION



NOTE:

2 NON-LIGHTED, YELLOW MESH CANVAS RUNWAY CLOSURE MARKERS PROVIDED BY THE AIRPORT, PLACE CLOSURE CROSSES IN THE CONFIGURATION SHOWN OVERTOP OF RUNWAY DESIGNATION MARKINGS (NUMERALS) OR IN THE ALTERNATE LOCATION SHOWN ON THE PLANS. CLOSED RUNWAY MARKERS SHALL BE RETURNED TO THE AIRPORT IN GOOD CONDITION AT THE END OF THE PROJECT.



1. AT THE BEGINNING OF THE PROJECT, CONTRACTOR WILL BE PROVIDED WITH TWO (2) TRAILER MOUNTED, LIGHTED RUNWAY CLOSURE MARKERS.
2. CONTRACTOR IS RESPONSIBLE FOR FUELING AND TRANSPORTING THE RUNWAY CLOSURE CROSSES FOR THE DURATION OF THE PROJECT.
3. AT THE END OF THE PROJECT, CONTRACTOR SHALL RETURN THE CLOSURE CROSSES TO THE AIRPORT IN GOOD WORKING CONDITION.

NON-LIGHTED

LIGHTED TRAILER MOUNTED

TYPICAL RUNWAY CLOSURE MARKERS

NO SCALE

A
02

GENERAL NOTES:

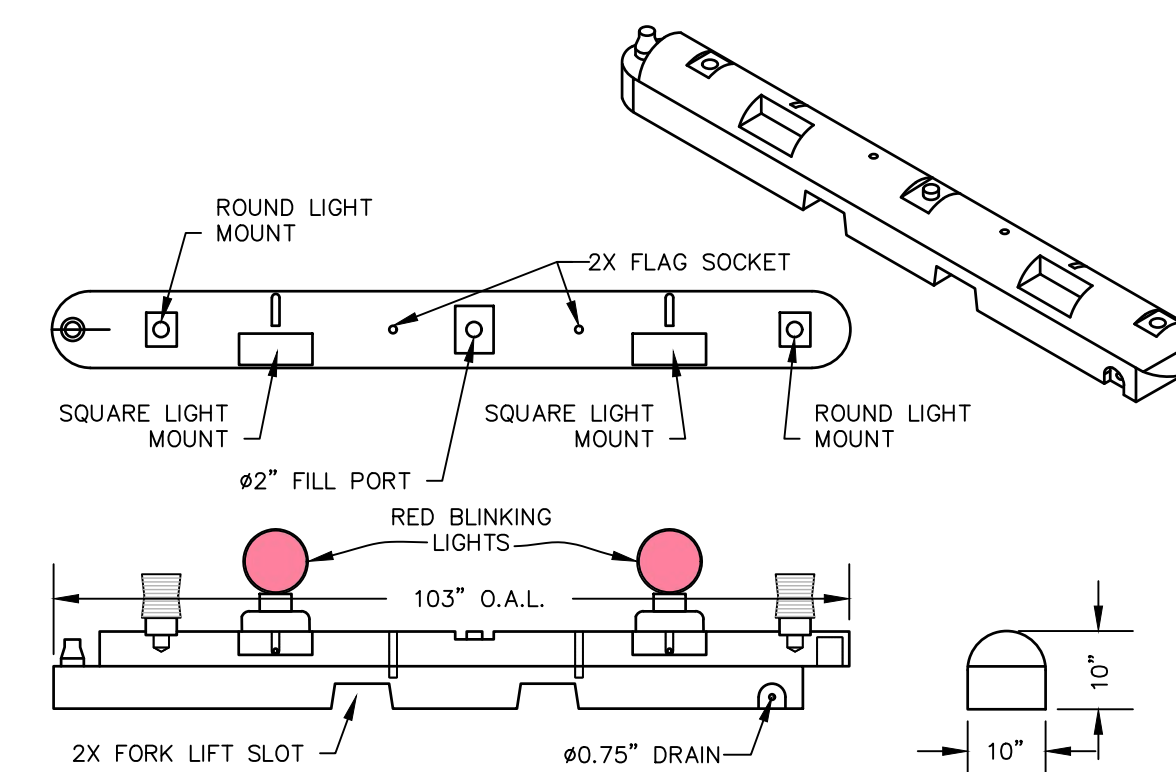
1. UNLESS STATED ELSEWHERE, BISHOP AIRPORT WILL REMAIN OPEN AT ALL TIMES DURING CONSTRUCTION.
2. RUNWAY CLOSED DOES NOT MEAN AIRPORT CLOSED.
3. THE BISHOP AIRPORT AIRBAND FREQUENCY IS 123.0 MHZ. WHILE ON THE AIRFIELD, THE CONTRACTOR SHALL APPOINT A RADIO MONITOR TASKED WITH MONITORING AND LISTENING TO THE RADIO AT ALL TIMES, WITH THE ABILITY TO GUIDE AND DIRECT CONTRACTOR FORCES IF NECESSARY. THE MONITOR SHALL REMAIN CLEAR OF ALL LOUD NOISE (MACHINE, EQUIPMENT) THAT COULD IMPEDE HIS ABILITY TO HEAR AND RESPOND TO RADIO COMMUNICATIONS.
4. BISHOP AIRPORT HAS THREE RUNWAYS: RUNWAY 12-30, RUNWAY 8-26, AND RUNWAY 17-35.
5. EACH RUNWAY INCLUDES ITS OWN RUNWAY SAFETY AREA (RSA).
6. CONTRACTOR FORCES TO REMAIN CLEAR OF THE RSA FOR EVERY RUNWAY UNLESS THAT SPECIFIC RUNWAY IS CLOSED.
7. PRIOR TO BEGINNING WORK ON THIS PROJECT, CONTRACTOR TO RECEIVE AIRPORT-OWNED LIGHTED RUNWAY CLOSURE CROSSES AND NON-LIGHTED RUNWAY CLOSURE CROSS PANELS. THE CONTRACTOR SHALL KEEP AND MAINTAIN ALL CROSSES FOR THE DURATION OF THE PROJECT. AT THE CONCLUSION OF THE WORK THE CONTRACTOR SHALL CLEAN, SERVICE (CHANGE OIL & FILTER - FUEL), NEATLY ROLL NON-LIGHTED PANELS, AND RETURN ALL TO THE AIRPORT.
8. NO EQUIPMENT OR MATERIALS PARKED OR STORED OUTSIDE THE DEFINED LIMITS OF THE CONTRACTOR YARD.
9. WATER FOR CONSTRUCTION IS NOT AVAILABLE AT BISHOP AIRPORT. CONTRACTOR TO SECURE THEIR OWN SOURCE OF WATER, OFF THE AIRPORT.
10. AIRPORT SECURITY PROGRAM:
 - A. CONTRACTOR FORCES TO PROCEED THROUGH BACKGROUND CHECK AND RECEIVE A SECURITY BADGE.
 - B. THE BADGE IS YOUR AUTHORITY TO ACCESS THE AIRPORT.
 - C. BADGED INDIVIDUALS CAN SERVE AS ESCORTS TO NON-BADGED INDIVIDUALS.
 - D. NON-BADGED INDIVIDUALS MUST REMAIN IN CLOSE RANGE (EAR-SHOT) OF THE BADGED ESCORT, AT ALL TIMES.
 - E. ALLOW 2-WEEKS MINIMUM TO SECURE A BADGE.

AIRPORT ACCESS CONTROL PROGRAM:

1. THROUGHOUT THE DURATION OF THIS PROJECT, THE INTEGRITY OF THE AIRPORT PERIMETER SECURITY ELEMENT SHALL REMAIN EQUAL TO OR GREATER THAN PRE-PROJECT CONDITIONS.
2. CONTRACTOR TO BE PREPARED TO DENY ACCESS TO THE AIRPORT TO ANYONE NOT UNDER THEIR CONTROL.
3. ALL VEHICLES AND EQUIPMENT ENTERING AND OPERATING ON THE AIRPORT SHALL HAVE EITHER AN ORANGE AND WHITE CHECKERED FLAG (DAYTIME ONLY) OR A FUNCTIONING AMBER ROTATING BEACON (DAY OR NIGHT) ON THE TOP OF THE VEHICLE, AT OR NEAR THE HIGHEST POINT.
4. MANUAL GATE ACCESS:
 - A. GATE TO BE CLOSED AND LOCKED AT ALL TIMES UNLESS WORKING IN PROXIMITY TO OR ON THE GATE.
 - B. LOCK YOURSELF IN, LOCK YOURSELF OUT.
 - C. NO DUMMY LOCKING THE GATE.
 - D. PROVIDE A SENTRY WHEN MOVING MULTIPLE LOADS, VEHICLES, EQUIPMENT, ETC., INTO OR OUT OF THE AIRPORT.
 - E. ANYONE WHO IS BADGED WILL RECEIVE A GATE CARD FOR GATE NO. 3.
5. AUTOMATIC GATE ACCESS:
 - A. USE YOUR GIVEN AUTHORITY TO ENTER AND EXIT THE AIRPORT.
 - B. CLEAR THE GATE WHEN MOVING IN EITHER DIRECTION AND REMAIN IN PROXIMITY UNTIL THE GATE COMES TO A COMPLETE CLOSE.
 - C. USE YOUR VEHICLE IF NECESSARY TO PROHIBIT UNAUTHORIZED AIRPORT ENTRY.
 - D. CONTRACTOR IS FULLY RESPONSIBLE FOR INDIVIDUALS, VEHICLES, AND EQUIPMENT THAT ENTER THE GATE BEHIND HIM.
 - E. ON MATERIAL IMPORT DAYS THE GATE WILL BE PARKED IN THE OPEN POSITION. PROVIDE A CONTINUOUSLY PRESENT SENTRY TO MONITOR AND CONTROL ACCESS.
 - F. DO NOT ATTEMPT TO ACCOMMODATE ACCESS OF VEHICLES OR INDIVIDUALS NOT IN YOUR CHARGE. ALL AUTHORIZED INDIVIDUALS WILL RESPECT YOUR EFFORTS TO PRESERVE AIRPORT PERIMETER SECURITY.
 - G. REPORT GATE ISSUES TO THE AIRPORT IMMEDIATELY.

RUNWAY CLOSURE NOTES:

1. CONTRACTOR FORCES TO REMAIN CLEAR (OUTSIDE) OF THE RUNWAY SAFETY AREA (RSA) UNLESS THE RUNWAY IS CLOSED.
2. AT LEAST ONE RUNWAY AT BISHOP AIRPORT WILL REMAIN OPEN AT ALL TIMES.
3. AUTHORIZED RUNWAY CLOSURE PERIODS:
 - A. RUNWAY 12-30: 24-HOURS A DAY, 7 DAYS A WEEK
 - B. RUNWAY 8-26: 0700 - 1600, LOCAL TIME, MONDAY - FRIDAY
 - C. RUNWAY 17-35: 2100 - 0600, LOCAL TIME, SUNDAY NIGHT - FRIDAY MORNING
 - D. NO HOLIDAY CLOSURES OF RUNWAY 8-26 AND RUNWAY 17-35.
4. CONTRACTOR TO SCHEDULE RUNWAY CLOSURES A MINIMUM OF SEVEN (7) DAYS IN ADVANCE AND CONFIRM A NOTAM HAS BEEN ISSUED BEFORE PROCEEDING.
5. CONTRACTOR TO INSTALL AND MAINTAIN RUNWAY CLOSURE CROSSES (LIGHTED AND / OR NON-LIGHTED, AS SHOWN) FOR THE DURATION OF EACH CLOSURE.
6. FOR NON-LIGHTED CLOSURE CROSS PANELS, CONTRACTOR SHALL PROVIDE ADEQUATE WEIGHTS (DELINATOR BASES) TO HOLD PANELS TO THE GROUND IN THE CROSS CONFIGURATION.
7. CONTRACTOR TO INSTALL AND MAINTAIN TAXIWAY CLOSURE DELINEATION AT LOCATIONS SHOWN, TO PREVENT AIRCRAFT FROM ACCESSING CLOSED RUNWAYS. DELINEATION TO BE PRESERVED AND MAINTAINED FOR THE DURATION OF EACH CLOSURE.
8. RUNWAYS AND ASSOCIATED CLOSED TAXIWAYS SHALL BE INSPECTED AND RE-OPENED BEFORE THE END OF EACH AUTHORIZED CLOSURE PERIOD. ALLOW AMPLE TIME FOR RE-SWEEPING AIRFIELD PAVEMENTS BEFORE RE-OPENING.

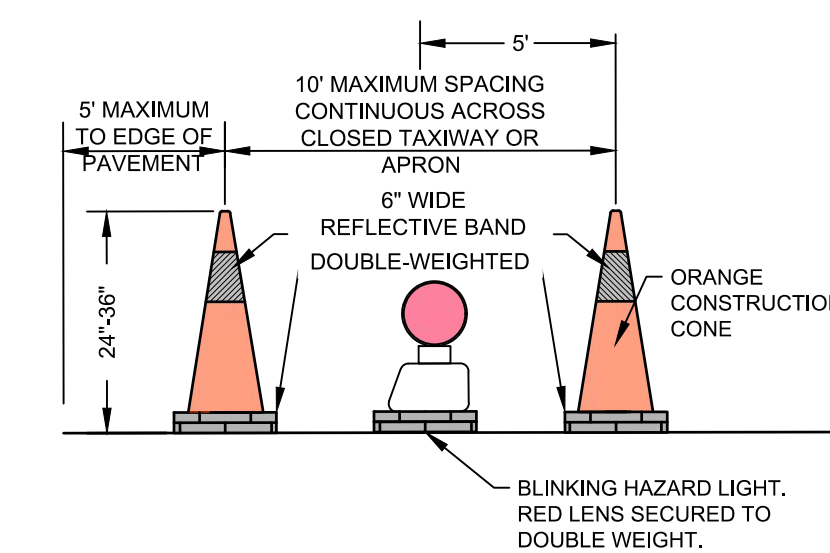


1. CONTRACTOR CAN ELECT EITHER 3 LED/SOLAR OR TWO BATTERY POWER LIGHTS PER DELINEATOR.
2. DELINEATORS TO BE PLACED EITHER INTERLOCKED OR SPACED WITH A 10\"/>

LOW PROFILE LIGHTED DELINEATOR DETAIL

NO SCALE

B
02



CONE DELINEATION DETAIL

NO SCALE

C
02

REFERENCE NOTES:

- 1 AREA OF WORK
- 2 CONTRACTOR YARD, MATERIAL AND EQUIPMENT STORAGE
- 3 CONTRACTOR POINT OF ACCESS TO AIRPORT
- 4 CONTRACTOR PATH OF TRAVEL

UNDERGROUND SERVICE ALERT
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 TWO WORKING DAYS BEFORE YOU DIG

PROJECT LAYOUT PLAN
 OVERALL VIEW

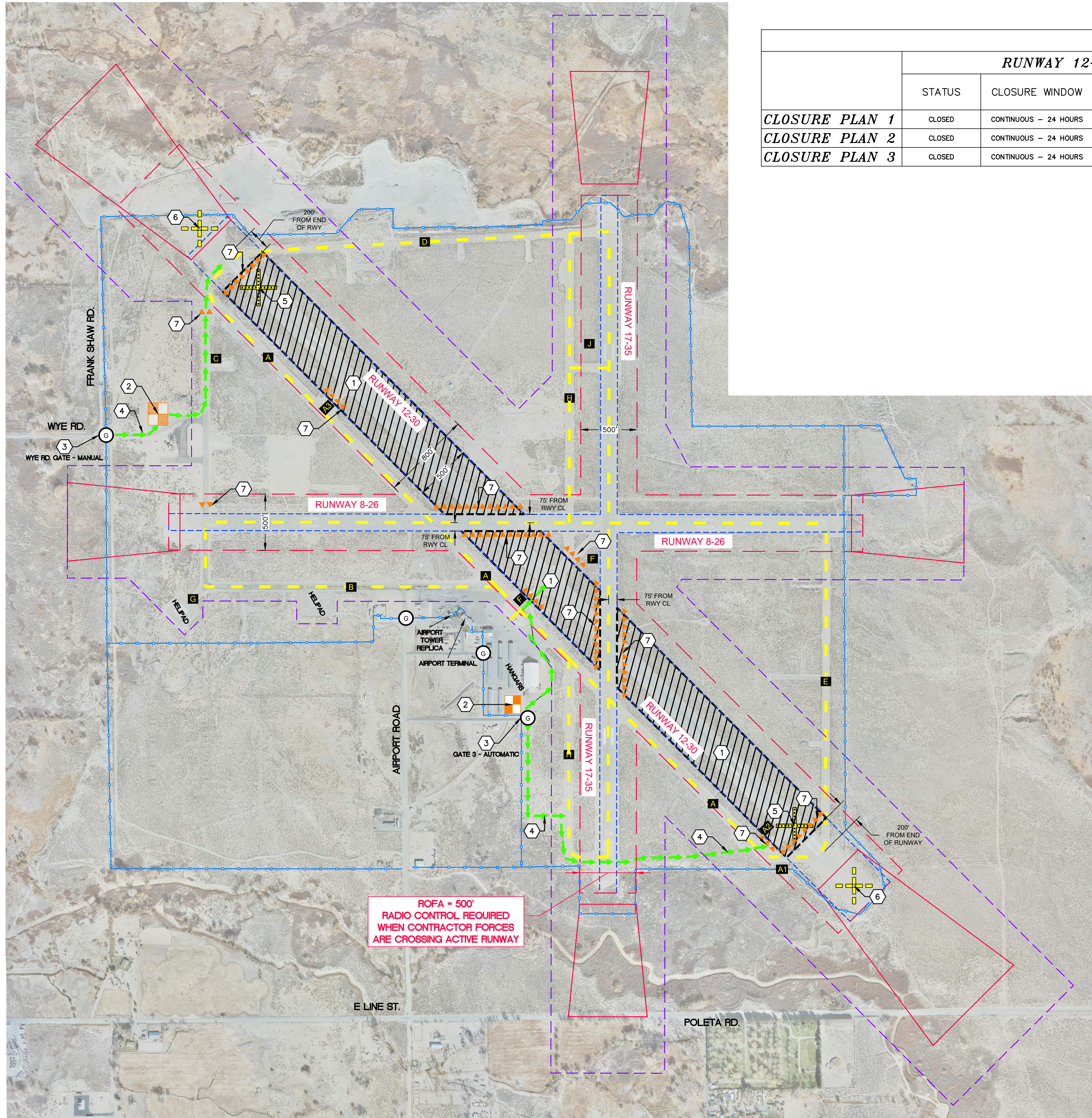
REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

REGISTERED PROFESSIONAL ENGINEER
 JACOB A. SMITH
 No. 46852
 EXP. 6-30-25
 CIVIL
 STATE OF CALIFORNIA

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 05/13/2024
 SHEET 02 of 12

PROJECT DATE: 06/26/2024



	RUNWAY 12-30			RUNWAY 8-26			RUNWAY 17-35		
	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES
CLOSURE PLAN 1	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	OPEN	N/A	N/A
CLOSURE PLAN 2	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	CLOSED	DAY SHIFT	10 SHIFTS	OPEN	N/A	N/A
CLOSURE PLAN 3	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	CLOSED	NIGHT SHIFT	10 SHIFTS

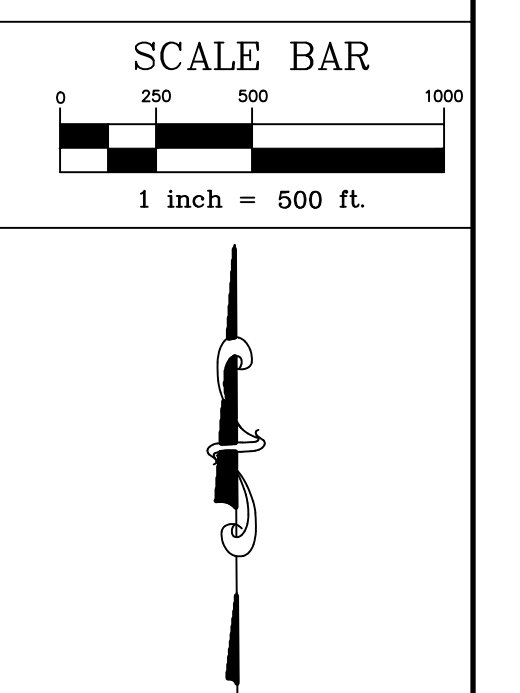
WITH ADVANCE NOTICE TO THE AIRPORT, THE CONTRACTOR IS TO IMPLEMENT CLOSURE PLAN 1, 2, OR 3 AT HIS DISCRETION, WITHIN THE CONSTRAINED NUMBER OF SHIFTS ALLOCATED FOR EACH CLOSURE PLAN, AS NECESSARY TO COMPLETE THE WORK.

REFERENCE NOTES (SHEETS 3-5):

- ① AREA OF WORK
- ② CONTRACTOR YARD, MATERIAL AND EQUIPMENT STORAGE
- ③ CONTRACTOR POINT OF ACCESS TO AIRPORT
- ④ CONTRACTOR PATH OF TRAVEL
- ⑤ LIGHTED CLOSURE CROSSES
- ⑥ LIGHTED CLOSURE CROSSES - ALTERNATE LOCATION
- ⑦ TAXIWAY CLOSURE DELINEATION, IN ACCORDANCE WITH DETAIL B OR C, SHEET 2.
- ⑧ NON-LIGHTED CLOSURE CROSSES

LEGEND (SHEETS 3-5):

- FENCE LINE
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- BUILDING RESTRICTION LINE
- RUNWAY PROTECTION ZONE
- ▨ WORK AREA
- ▨ CONTRACTOR YARD
- CONTRACTOR PATH OF TRAVEL
- ⊙ AIRPORT ENTRANCE GATE
- ⊕ LIGHTED CLOSURE CROSS
- ⊕ LIGHTED CLOSURE CROSS-SECONDARY LOCATION
- ⊕ NON-LIGHTED CLOSURE CROSS
- A TAXIWAY DESIGNATION
- ▲ PAVEMENT CLOSED DELINEATION
- AIRCRAFT PATH OF TRAVEL



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CLOSURE PLAN 1

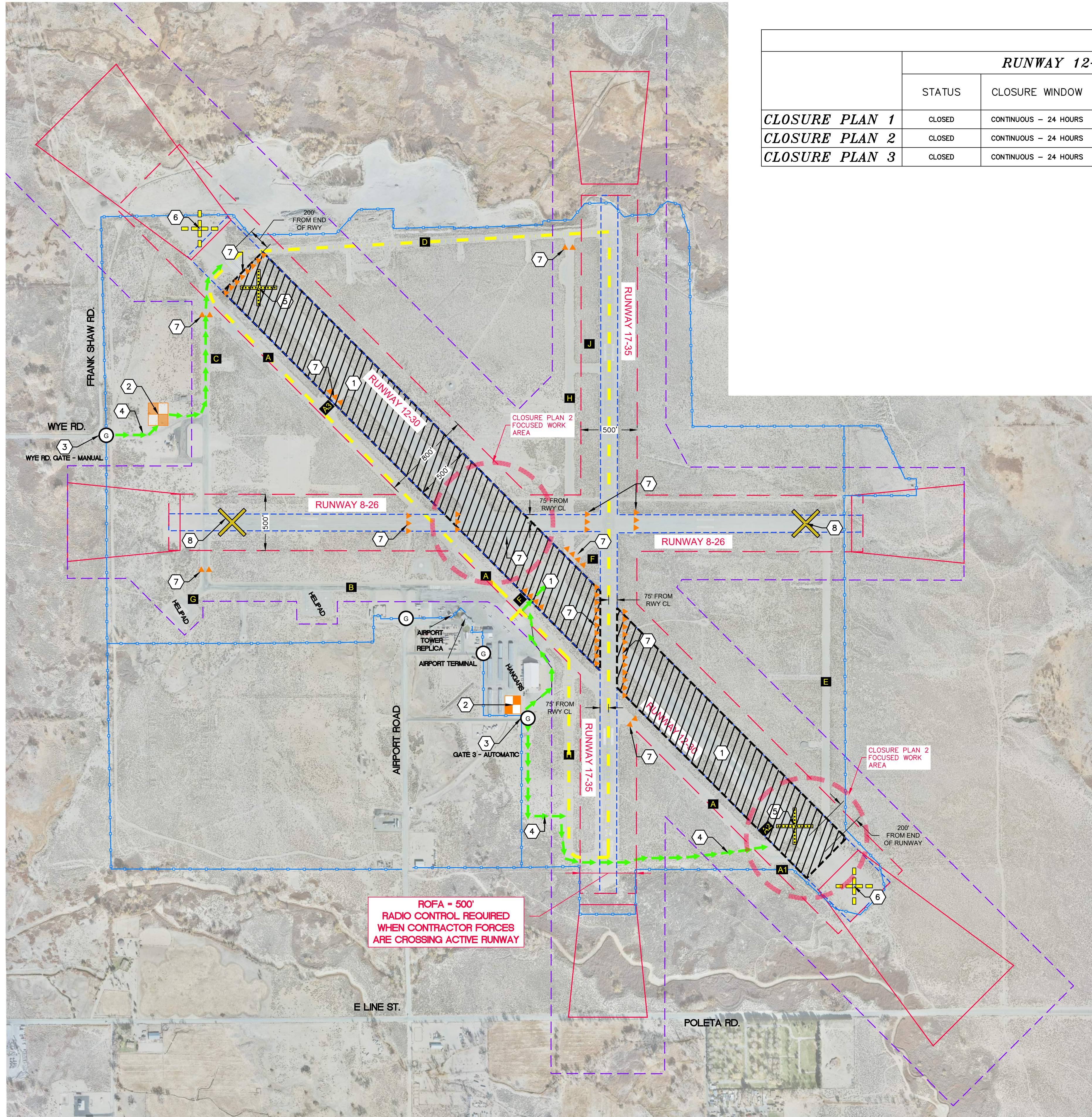
RUNWAY 12-30 CLOSED

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 05/13/2024
 SHEET 03 of 12

REGISTERED PROFESSIONAL ENGINEER
 JACOB A. SMITH
 No. 46852
 EXP. 6-30-25
 CIVIL
 STATE OF CALIFORNIA



RUNWAY CLOSURE PLAN OVERVIEW									
	RUNWAY 12-30			RUNWAY 8-26			RUNWAY 17-35		
	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES
CLOSURE PLAN 1	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	OPEN	N/A	N/A
CLOSURE PLAN 2	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	CLOSED	DAY SHIFT	10 SHIFTS	OPEN	N/A	N/A
CLOSURE PLAN 3	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	CLOSED	NIGHT SHIFT	10 SHIFTS

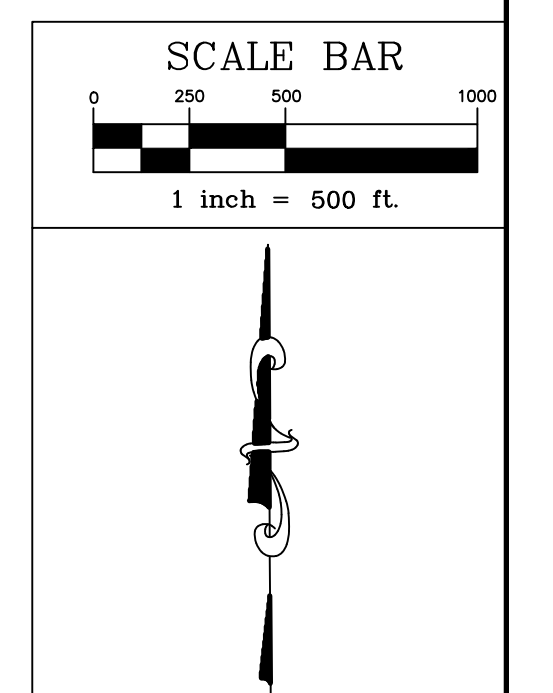
WITH ADVANCE NOTICE TO THE AIRPORT, THE CONTRACTOR IS TO IMPLEMENT CLOSURE PLAN 1, 2, OR 3 AT HIS DISCRETION, WITHIN THE CONSTRAINED NUMBER OF SHIFTS ALLOCATED FOR EACH CLOSURE PLAN, AS NECESSARY TO COMPLETE THE WORK.

REFERENCE NOTES (SHEETS 3-5):

- ① AREA OF WORK
- ② CONTRACTOR YARD, MATERIAL AND EQUIPMENT STORAGE
- ③ CONTRACTOR POINT OF ACCESS TO AIRPORT
- ④ CONTRACTOR PATH OF TRAVEL
- ⑤ LIGHTED CLOSURE CROSSES
- ⑥ LIGHTED CLOSURE CROSSES - ALTERNATE LOCATION
- ⑦ TAXIWAY CLOSURE DELINEATION, IN ACCORDANCE WITH DETAIL B OR C, SHEET 2.
- ⑧ NON-LIGHTED CLOSURE CROSSES

LEGEND (SHEETS 3-5):

- FENCE LINE
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- BUILDING RESTRICTION LINE
- RUNWAY PROTECTION ZONE
- ||||| WORK AREA
- ▣ CONTRACTOR YARD
- CONTRACTOR PATH OF TRAVEL
- ⊙ AIRPORT ENTRANCE GATE
- ⊕ LIGHTED CLOSURE CROSS
- ⊕ LIGHTED CLOSURE CROSS-SECONDARY LOCATION
- ⊕ NON-LIGHTED CLOSURE CROSS
- A TAXIWAY DESIGNATION
- ▲ PAVEMENT CLOSED DELINEATION
- AIRCRAFT PATH OF TRAVEL



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 TWO WORKING DAYS BEFORE YOU DIG

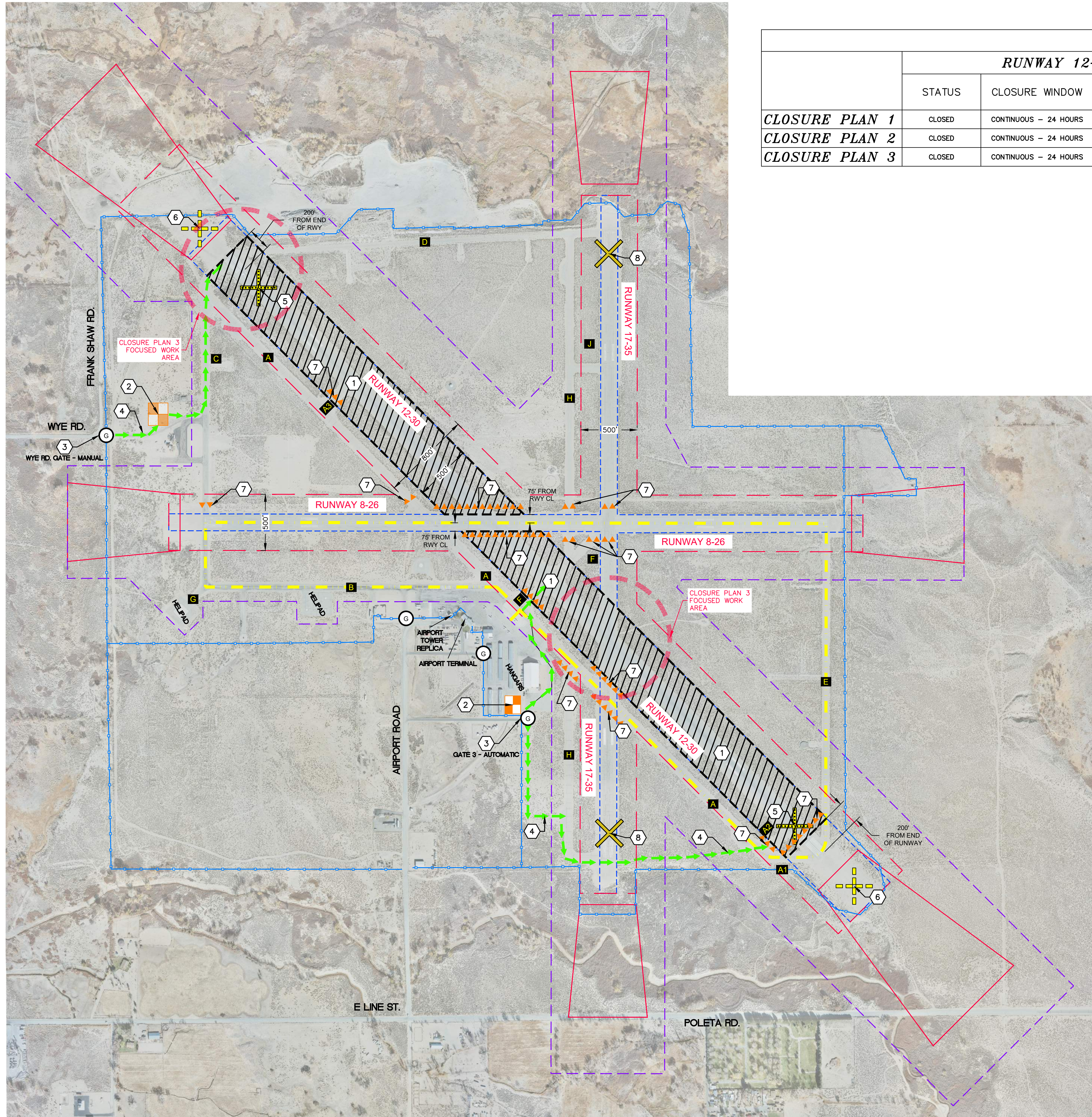
CLOSURE PLAN 2

RUNWAY 12-30 & RUNWAY 8-26 CLOSED

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
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 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 05/13/2024
 SHEET 04 of 12



RUNWAY CLOSURE PLAN OVERVIEW									
	RUNWAY 12-30			RUNWAY 8-26			RUNWAY 17-35		
	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES	STATUS	CLOSURE WINDOW	MAXIMUM ALLOWABLE CLOSURES
CLOSURE PLAN 1	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	OPEN	N/A	N/A
CLOSURE PLAN 2	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	CLOSED	DAY SHIFT	10 SHIFTS	OPEN	N/A	N/A
CLOSURE PLAN 3	CLOSED	CONTINUOUS - 24 HOURS	NO RESTRICTIONS	OPEN	N/A	N/A	CLOSED	NIGHT SHIFT	10 SHIFTS

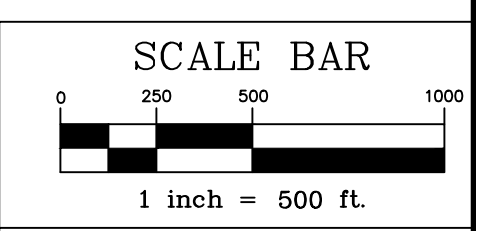
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REFERENCE NOTES (SHEETS 3-5):

- ① AREA OF WORK
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- ④ CONTRACTOR PATH OF TRAVEL
- ⑤ LIGHTED CLOSURE CROSSES
- ⑥ LIGHTED CLOSURE CROSSES - ALTERNATE LOCATION
- ⑦ TAXIWAY CLOSURE DELINEATION, IN ACCORDANCE WITH DETAIL B OR C, SHEET 2.
- ⑧ NON-LIGHTED CLOSURE CROSSES

LEGEND (SHEETS 3-5):

- FENCE LINE
- RUNWAY SAFETY AREA
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- BUILDING RESTRICTION LINE
- RUNWAY PROTECTION ZONE
- ▨ WORK AREA
- ▨ CONTRACTOR YARD
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- ⊙ AIRPORT ENTRANCE GATE
- ⊕ LIGHTED CLOSURE CROSS
- ⊕ LIGHTED CLOSURE CROSS-SECONDARY LOCATION
- ⊕ NON-LIGHTED CLOSURE CROSS
- A TAXIWAY DESIGNATION
- ▲ PAVEMENT CLOSED DELINEATION
- AIRCRAFT PATH OF TRAVEL



UNDERGROUND SERVICE ALERT
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CLOSURE PLAN 3

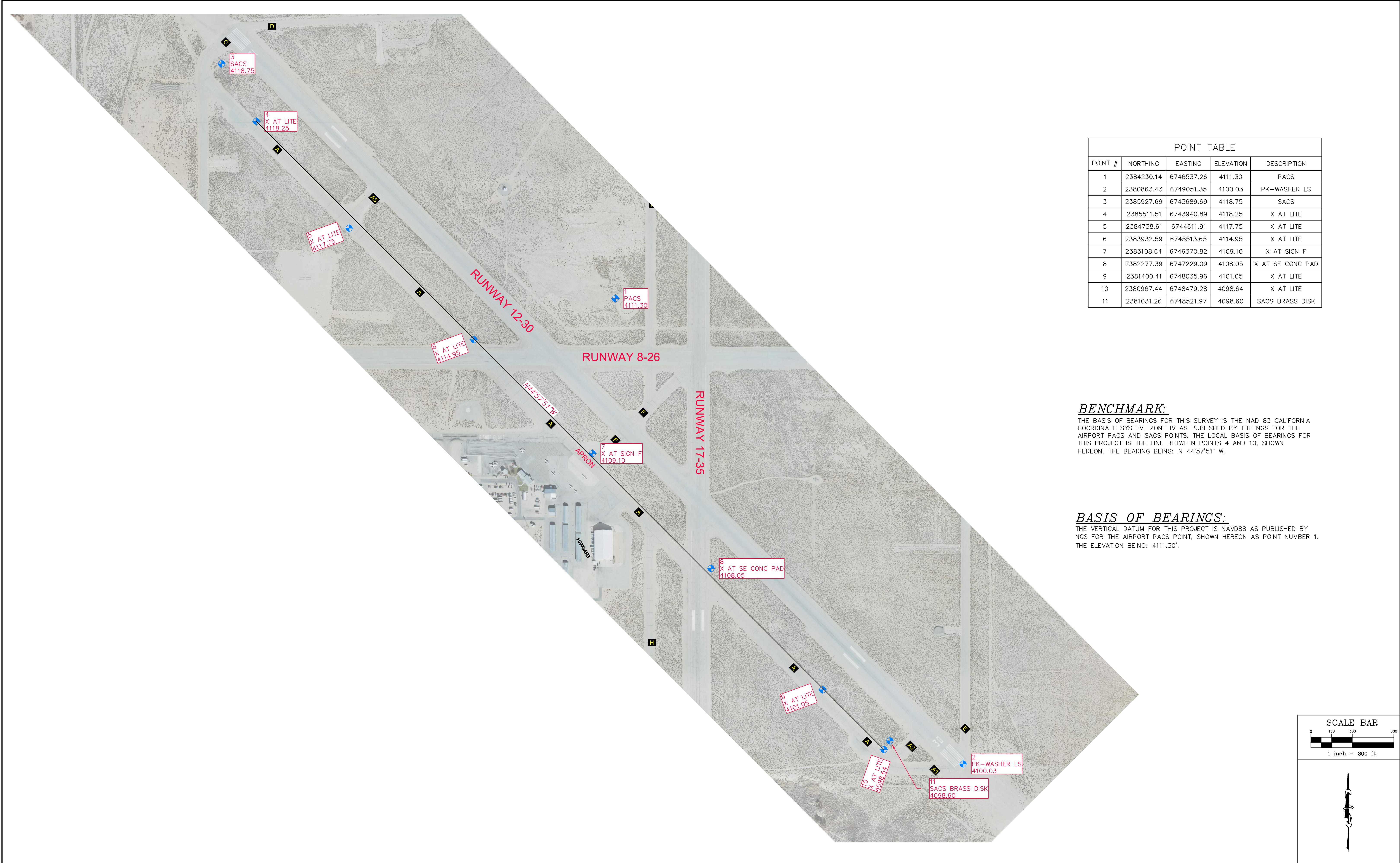
RUNWAY 12-30 & RUNWAY 17-35 CLOSED

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
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 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
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 DATE 05/13/2024
 SHEET 05 of 12

PL01 DAT16-2024



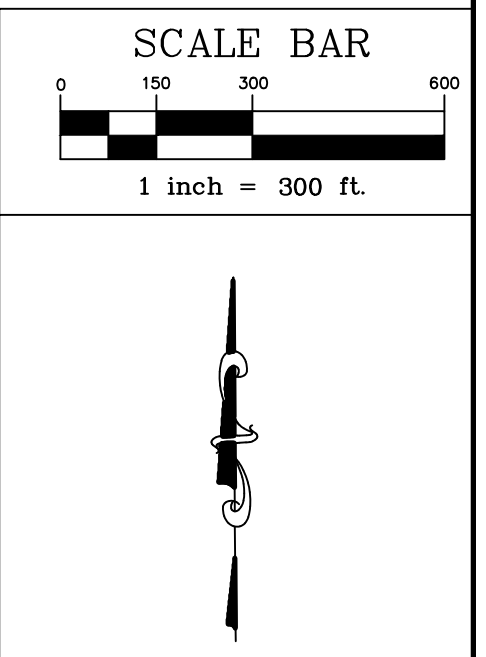
POINT TABLE				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	2384230.14	6746537.26	4111.30	PACS
2	2380863.43	6749051.35	4100.03	PK-WASHER LS
3	2385927.69	6743689.69	4118.75	SACS
4	2385511.51	6743940.89	4118.25	X AT LITE
5	2384738.61	6744611.91	4117.75	X AT LITE
6	2383932.59	6745513.65	4114.95	X AT LITE
7	2383108.64	6746370.82	4109.10	X AT SIGN F
8	2382277.39	6747229.09	4108.05	X AT SE CONC PAD
9	2381400.41	6748035.96	4101.05	X AT LITE
10	2380967.44	6748479.28	4098.64	X AT LITE
11	2381031.26	6748521.97	4098.60	SACS BRASS DISK


BENCHMARK:

THE BASIS OF BEARINGS FOR THIS SURVEY IS THE NAD 83 CALIFORNIA COORDINATE SYSTEM, ZONE IV AS PUBLISHED BY THE NGS FOR THE AIRPORT PACS AND SACS POINTS. THE LOCAL BASIS OF BEARINGS FOR THIS PROJECT IS THE LINE BETWEEN POINTS 4 AND 10, SHOWN HEREON. THE BEARING BEING: N 44° 57' 51\"/>

BASIS OF BEARINGS:

THE VERTICAL DATUM FOR THIS PROJECT IS NAVD88 AS PUBLISHED BY NGS FOR THE AIRPORT PACS POINT, SHOWN HEREON AS POINT NUMBER 1. THE ELEVATION BEING: 4111.30'.



UNDERGROUND SERVICE ALERT
 DIAL 811
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 BEFORE YOU DIG


CONTROL PLAN

BENCHMARK & BASIS OF BEARINGS

REV.	DESCRIPTION	DATE	APP.
1			

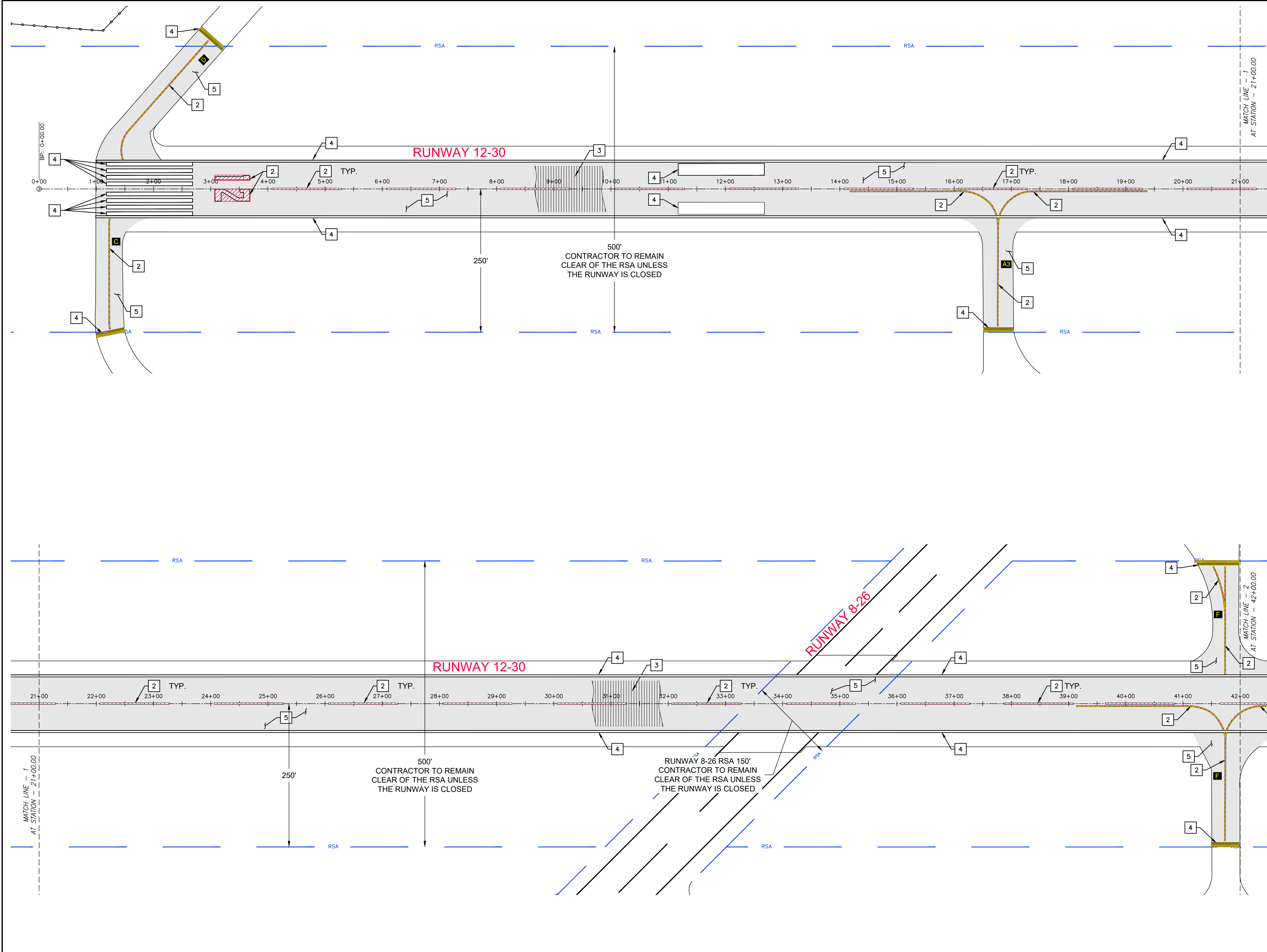
TARTAGLIA ENGINEERING
 BISHOP AIRPORT
A County of Inyo Aviation Facility

RUNWAY 12-30 SURFACE TREATMENT



DESIGN JTH
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 SHEET 08 of 12

PL01.DWG 6/26/2024



IMPROVEMENT NOTES
(SHEET 07-08):

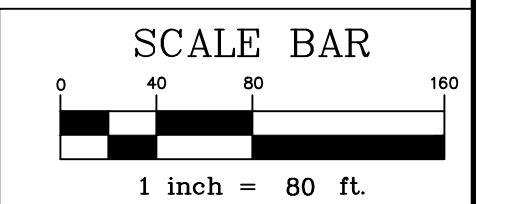
1. ROUTE, CLEAN, AND FILL CRACKS THROUGHOUT THE ENTIRE LENGTH OF RUNWAY 12-30 IN ACCORDANCE WITH DETAIL A, SHEET 11. (NOT SHOWN ON PLANS).
2. OBLITERATE PAVEMENT MARKING TO 50%.
3. PERFORM PAVEMENT GROOVING THROUGHOUT THE ENTIRE LENGTH OF RUNWAY 12-30 IN ACCORDANCE WITH DETAIL B, SHEET 11.
4. EXISTING PAVEMENT MARKING. SHIELD IN PLACE FROM ASPHALT EMULSION SEAL COAT.
5. APPLY ASPHALT EMULSION SEAL COAT TO RUNWAY 12-30 SURFACE AND EACH CONNECTING TAXIWAY UP TO THE HOLD POSITION MARKINGS.

CONSTRUCTION SEQUENCING NOTES:

1. CRACK REPAIR AND PAVEMENT MARKING REMOVAL EFFORT SHALL OCCUR BEFORE GROOVING OPERATIONS.
2. P-608 APPLICATION TO OCCUR AFTER ASPHALT HAS BEEN GROOVED.
3. CONTRACTOR TO WAIT A MINIMUM OF 48 HOURS AFTER P-608 APPLICATION TO APPLY FIRST COAT OF PAVEMENT MARKINGS.
5. A CONTRACT TIME SUSPENSION WILL BE ISSUED AFTER COMPLETION OF THE FIRST COAT OF PAVEMENT MARKINGS.
6. SECOND COAT OF PAVEMENT MARKINGS TO BE APPLIED A MINIMUM OF 30 CALENDAR DAYS AFTER THE P-608 APPLICATION.

LEGEND:

- RSA RUNWAY SAFETY AREA
- AREA OF RUNWAY GROOVING
- PROPOSED ASPHALT EMULSION SEAL COAT
- TAXIWAY DESIGNATION
- PAVEMENT MARKING REMOVAL



UNDERGROUND SERVICE ALERT
 DIAL 811
 TWO WORKING DAYS BEFORE YOU DIG

IMPROVEMENT PLAN

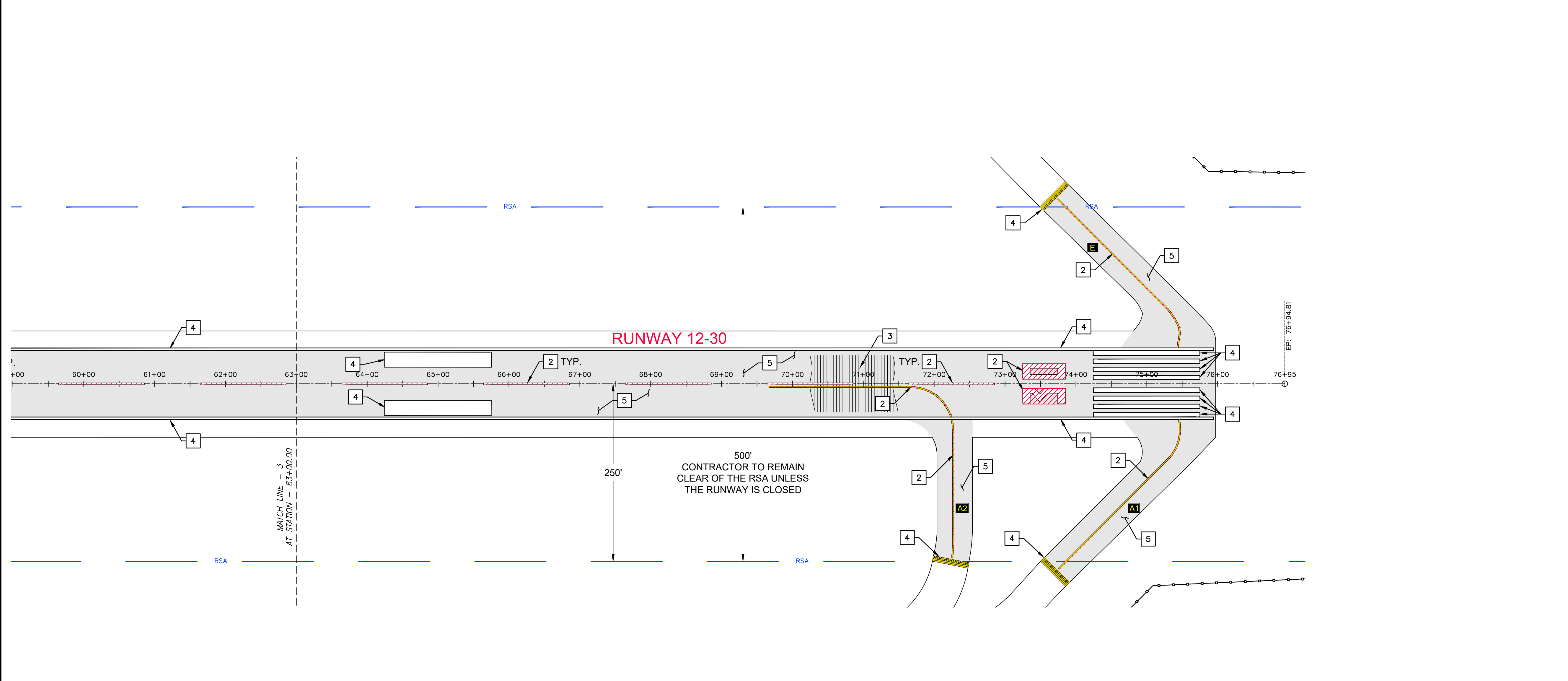
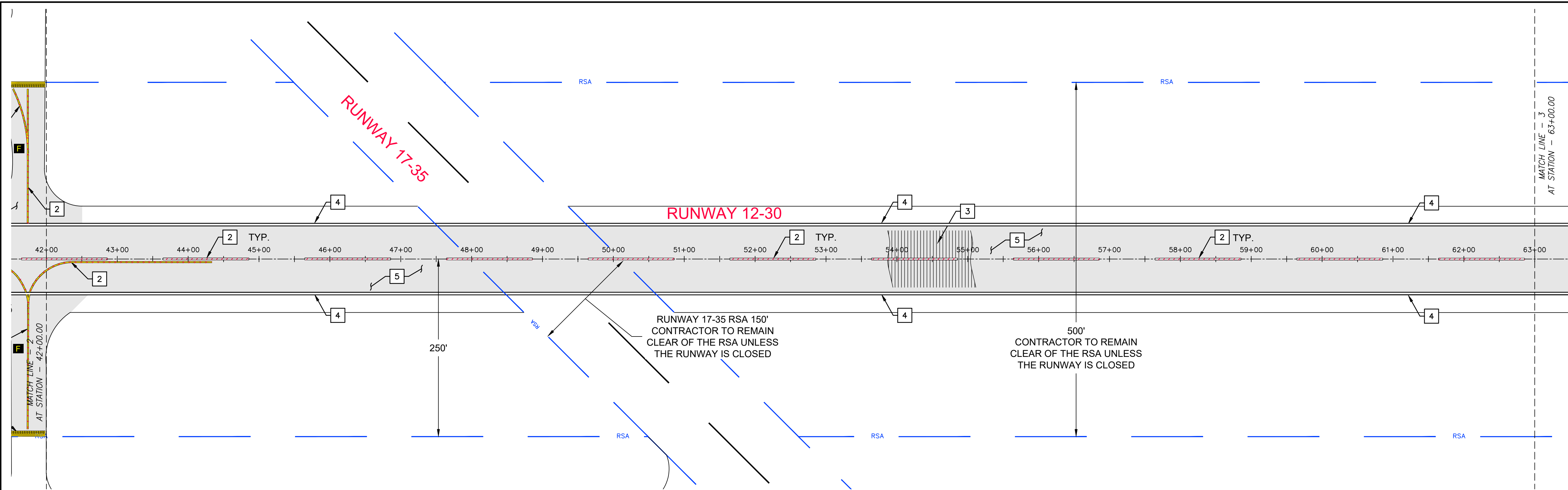
RUNWAY 12-30 STA: 0+00 TO STA. 42+00

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 05/13/2024
 SHEET 07 of 12

PLOT DATE: 6/26/2024



IMPROVEMENT NOTES
(SHEET 07-08):

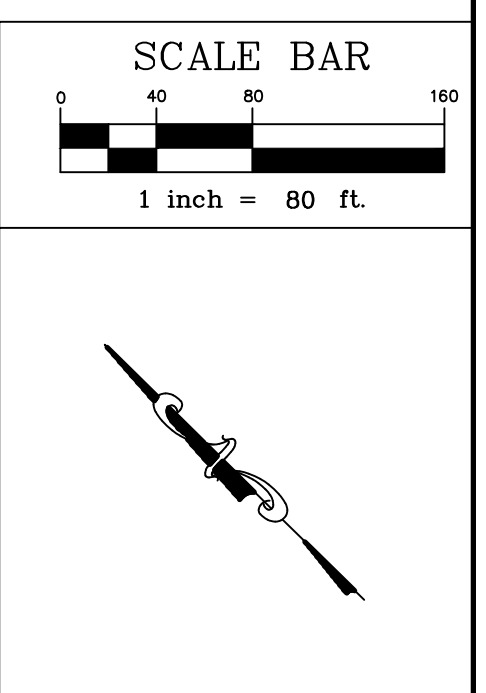
1. ROUTE, CLEAN, AND FILL CRACKS THROUGHOUT THE ENTIRE LENGTH OF RUNWAY 12-30 IN ACCORDANCE WITH DETAIL A, SHEET 11. (NOT SHOWN ON PLANS).
2. OBLITERATE PAVEMENT MARKING TO 50%.
3. PERFORM PAVEMENT GROOVING THROUGHOUT THE ENTIRE LENGTH OF RUNWAY 12-30 IN ACCORDANCE WITH DETAIL B, SHEET 11.
4. EXISTING PAVEMENT MARKING. SHIELD IN PLACE FROM ASPHALT EMULSION SEAL COAT.
5. APPLY ASPHALT EMULSION SEAL COAT TO RUNWAY 12-30 SURFACE AND EACH CONNECTING TAXIWAY UP TO THE HOLD POSITION MARKINGS.

CONSTRUCTION SEQUENCING NOTES:

1. CRACK REPAIR AND PAVEMENT MARKING REMOVAL EFFORT SHALL OCCUR BEFORE GROOVING OPERATIONS.
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5. A CONTRACT TIME SUSPENSION WILL BE ISSUED AFTER COMPLETION OF THE FIRST COAT OF PAVEMENT MARKINGS.
6. SECOND COAT OF PAVEMENT MARKINGS TO BE APPLIED A MINIMUM OF 30 CALENDAR DAYS AFTER THE P-608 APPLICATION.

LEGEND:

- RSA RUNWAY SAFETY AREA
- AREA OF RUNWAY GROOVING
- PROPOSED ASPHALT EMULSION SEAL COAT
- TAXIWAY DESIGNATION
- PAVEMENT MARKING REMOVAL



UNDERGROUND SERVICE ALERT
 DIAL 811
 TWO WORKING DAYS BEFORE YOU DIG

IMPROVEMENT PLAN

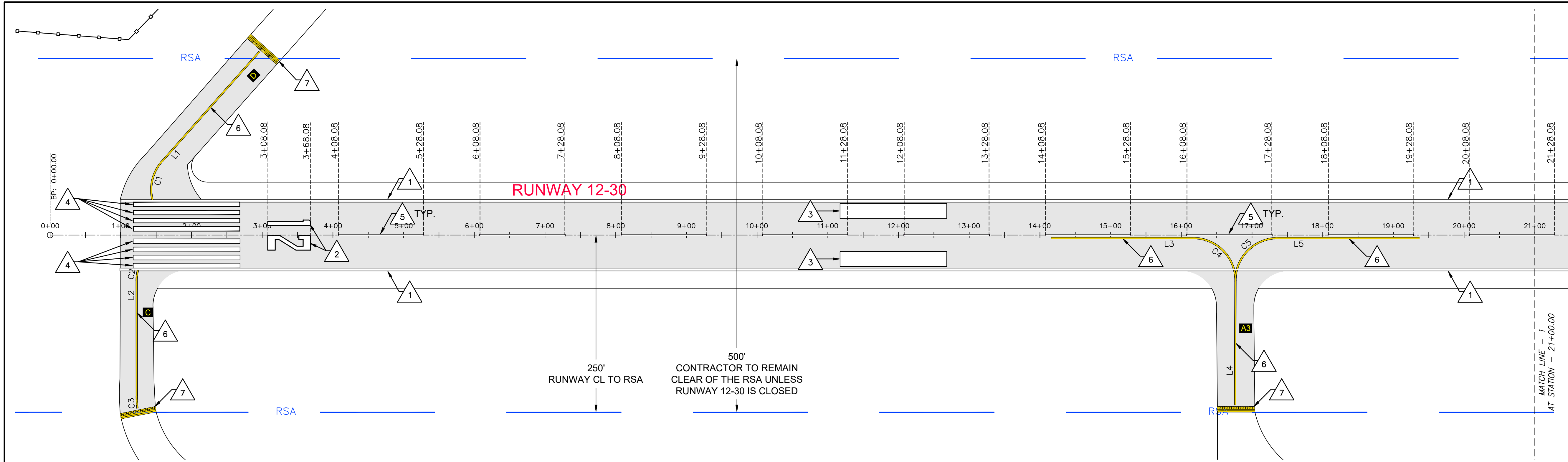
RUNWAY 12-30 STA: 42+00 TO STA. 76+94.81

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 05/13/2024
 SHEET 08 of 12

PLOT DATE: 6/26/2024



PAVEMENT MARKING NOTES (SHEETS 09-10):

1. APPLY TWO (2) COAT RUNWAY EDGE MARKING, 36-INCH WIDE WHITE WITH 6-INCH BLACK BORDER, IN ACCORDANCE WITH DETAILS A AND B, SHEET 12.
2. APPLY TWO (2) COAT RUNWAY DESIGNATOR MARKING (NUMBER) WITH BLACK BORDER OVER EXISTING IN ACCORDANCE WITH DETAIL E AND DETAIL F OR G, SHEET 12.
3. APPLY TWO (2) COAT RUNWAY AIMING POINT MARKING, WHITE WITH BLACK BORDER, OVER EXISTING IN ACCORDANCE WITH DETAIL D, SHEET 12.
4. APPLY TWO (2) COAT RUNWAY THRESHOLD MARKINGS (PIANO KEYS) OVER EXISTING IN ACCORDANCE WITH DETAIL F OR G, SHEET 12.
5. APPLY TWO (2) COAT RUNWAY CENTERLINE MARKING IN ACCORDANCE WITH DETAILS A AND B, SHEET 12.
6. APPLY TWO COAT TAXIWAY CENTERLINE AND LEAD IN LINE MARKINGS WITH BLACK BORDER IN ACCORDANCE WITH DETAILS C, H, AND I, SHEET 12.
7. APPLY TWO (2) COAT RUNWAY HOLD POSITION MARKING OVER EXISTING IN ACCORDANCE WITH DETAIL J, SHEET 12.

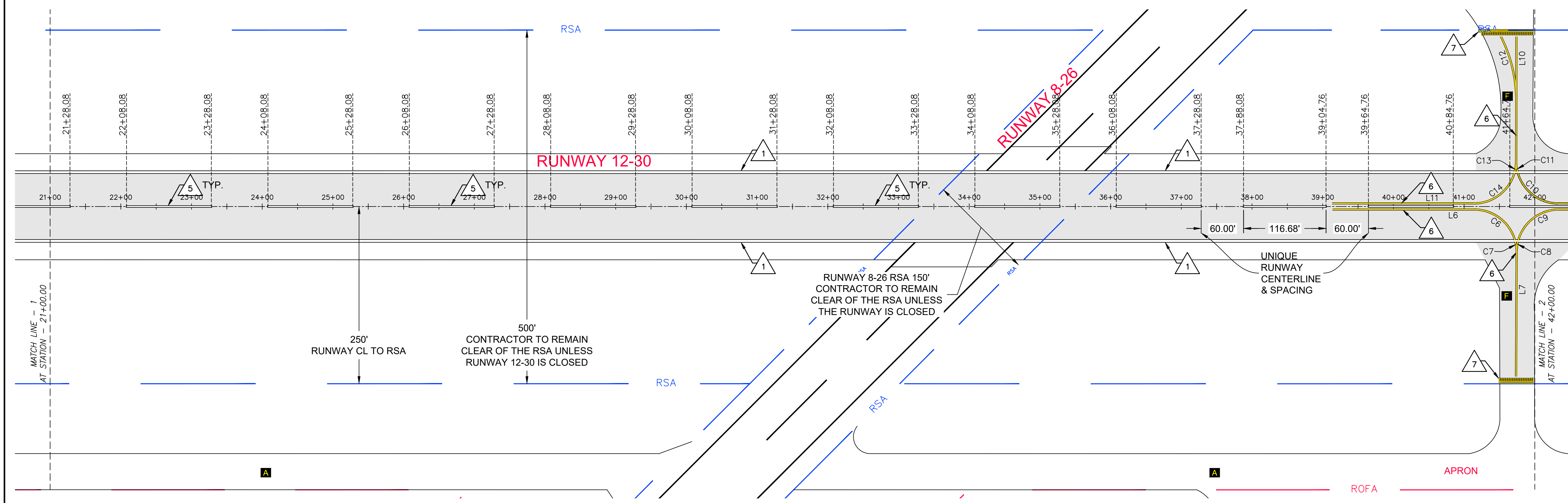
LINE #	LENGTH	DIRECTION
L1	207.48	S86° 43' 40"W
L2	163.89	N45° 04' 36"E
L3	200.00	N44° 53' 18"W
L4	175.97	N45° 16' 30"E
L5	200.00	S44° 53' 18"E

LINE #	LENGTH	DIRECTION
L6	200.00	N44° 53' 18"W
L7	175.43	S45° 22' 27"W
L8	200.00	S44° 53' 18"E
L9	200.00	S44° 53' 18"E
L10	175.69	S45° 14' 35"W

LINE #	LENGTH	DIRECTION
L11	200.00	N44° 53' 18"W
L12	200.00	N44° 53' 18"W
L13	163.43	S45° 12' 02"W
L14	218.34	S89° 56' 22"E
L15	217.25	S0° 06' 36"W

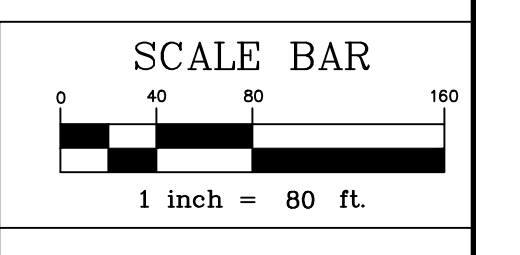
CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C1	57.94	60.00	55.33	S59° 03' 49"W	55.71
C2	14.24	60.00	13.60	N51° 52' 39"E	14.21
C3	16.56	150.00	6.32	N41° 36' 00"E	16.55
C4	76.96	60.00	73.49	N8° 08' 37"W	71.79
C5	76.96	60.00	73.49	S81° 37' 59"E	71.79
C6	77.00	60.00	73.53	N8° 07' 30"W	71.82
C7	14.42	60.00	13.77	S38° 29' 22"W	14.39

CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C8	13.87	60.00	13.24	S51° 59' 48"W	13.84
C9	77.00	60.00	73.53	S81° 39' 06"E	71.82
C10	76.83	60.00	73.37	S8° 12' 13"E	71.69
C11	14.44	60.00	13.79	S38° 20' 46"W	14.41
C12	93.55	200.00	26.80	N31° 42' 43"E	92.70
C13	14.17	60.00	13.53	N52° 00' 31"E	14.14
C14	76.83	60.00	73.37	N81° 34' 22"W	71.69



GENERAL PAVEMENT MARKING NOTES (SHEETS 09-10):

1. SURFACE TO BE DRY AND CLEAR OF ALL VEGETATION, ROCK, LOOSE FINES, ETC., BEFORE APPLYING MARKINGS.
2. ONE HUNDRED PERCENT OF MATERIAL REMOVED FROM THE SURFACE (PAINT, FOD, SILT, RUBBER, ETC.), INCLUDING MEDIA USED IN THE REMOVAL MUST BE ACCUMULATED, CONTAINED, AND PROPERLY DISPOSED OF OFF AIRPORT PROPERTY.
3. BLACK BORDERS REQUIRED ON ALL RUNWAY MARKINGS.
4. ALL MARKINGS ARE TWO (2) COAT. NO BEADS IN FIRST COAT.



UNDERGROUND SERVICE ALERT
 DIAL 811
 TWO WORKING DAYS BEFORE YOU DIG

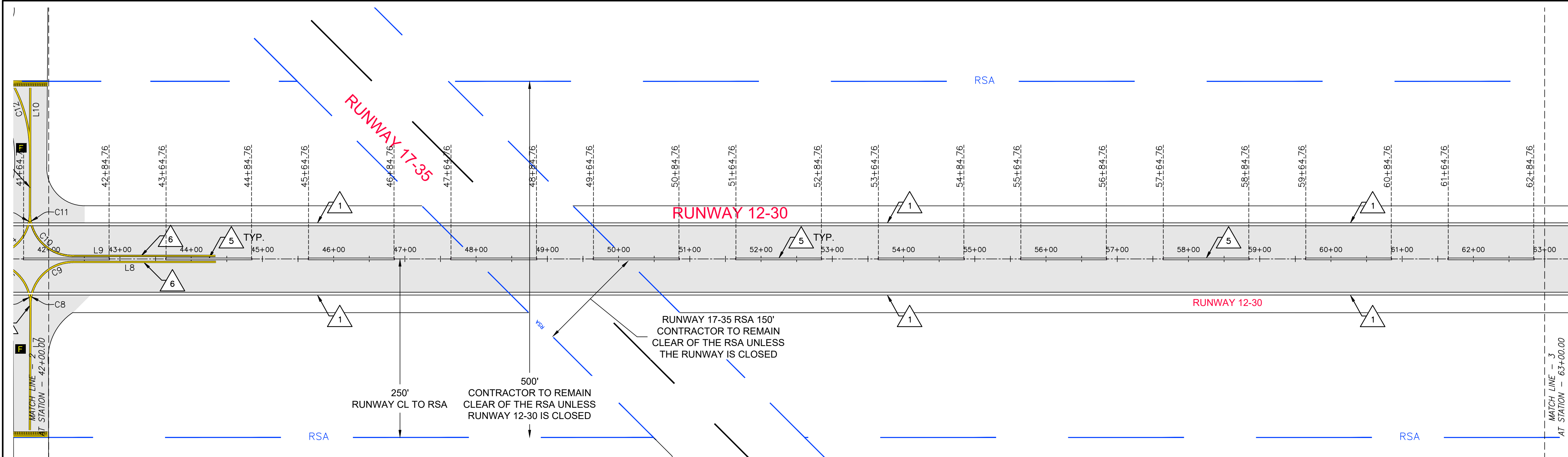
MARKING PLAN
 RUNWAY 12-30 STA: 0+00 TO STA. 42+00

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 06/18/2024
 SHEET 09 of 12

PL01 DATE: 6/26/2024



PAVEMENT MARKING NOTES (SHEETS 09-10):

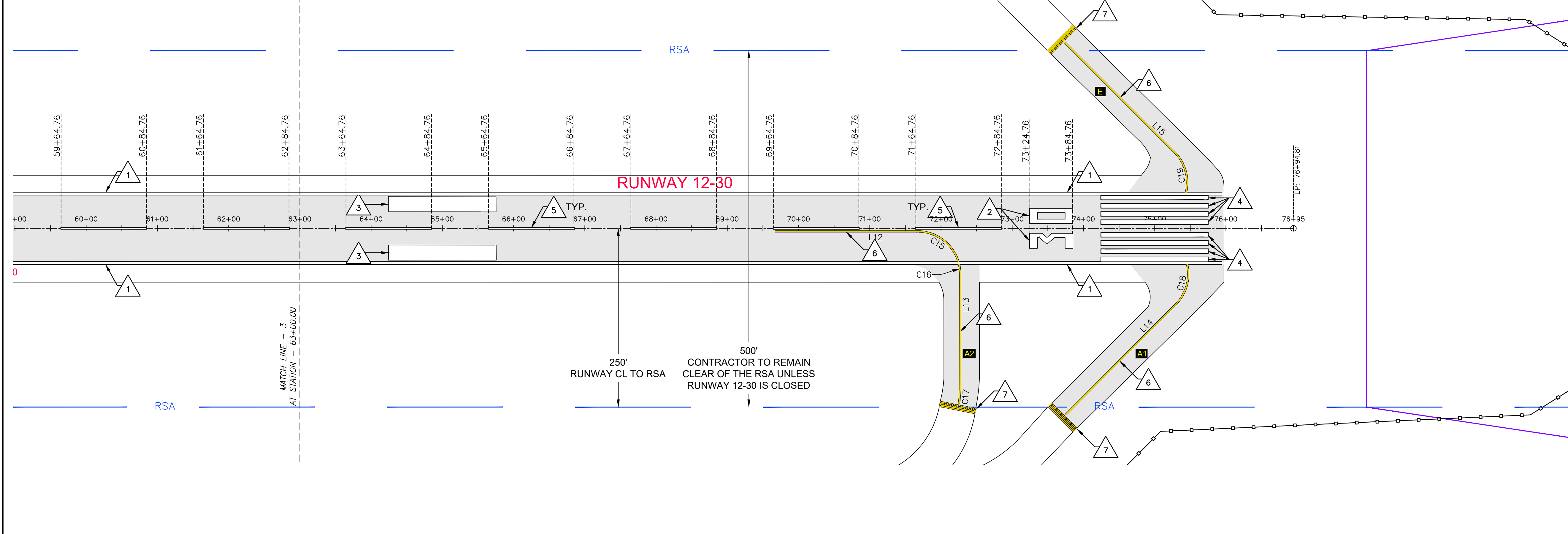
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3. APPLY TWO (2) COAT RUNWAY AIMING POINT MARKING, WHITE WITH BLACK BORDER, OVER EXISTING IN ACCORDANCE WITH DETAIL D, SHEET 12.
4. APPLY TWO (2) COAT RUNWAY THRESHOLD MARKINGS (PIANO KEYS) OVER EXISTING IN ACCORDANCE WITH DETAIL F OR G, SHEET 12.
5. APPLY TWO (2) COAT RUNWAY CENTERLINE MARKING IN ACCORDANCE WITH DETAILS A AND B, SHEET 12.
6. APPLY TWO COAT TAXIWAY CENTERLINE AND LEAD IN LINE MARKINGS WITH BLACK BORDER IN ACCORDANCE WITH DETAILS C, H, AND I, SHEET 12.
7. APPLY TWO (2) COAT RUNWAY HOLD POSITION MARKING OVER EXISTING IN ACCORDANCE WITH DETAIL J, SHEET 12.

LINE #	LENGTH	DIRECTION
L6	200.00	N44° 53' 18"W
L7	175.43	S45° 22' 27"W
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L9	200.00	S44° 53' 18"E
L10	175.69	S45° 14' 35"W

LINE #	LENGTH	DIRECTION
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L13	163.43	S45° 12' 02"W
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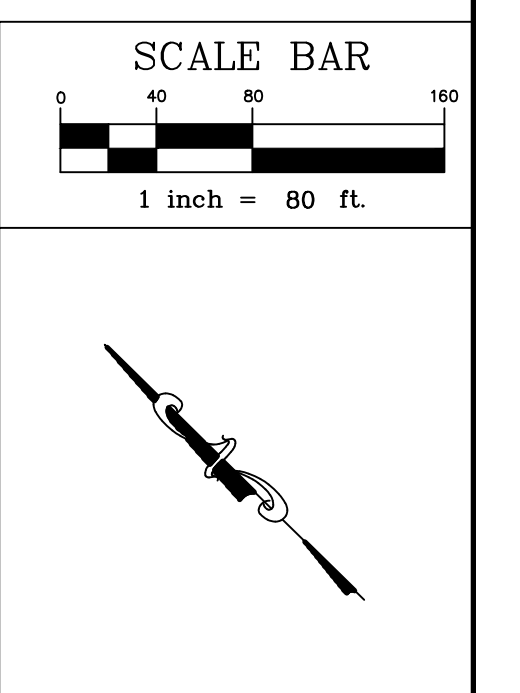
CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
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C13	14.17	60.00	13.53	N52° 00' 31"E	14.14
C14	76.83	60.00	73.37	N81° 34' 22"W	71.69

CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C15	77.04	60.00	73.57	N8° 06' 08"W	71.86
C16	14.19	60.00	13.55	S38° 25' 30"W	14.16
C17	17.57	150.00	6.71	S49° 40' 21"W	17.56
C18	61.16	60.00	58.41	N60° 51' 26"E	58.55
C19	61.39	60.00	58.62	S29° 25' 17"W	58.75



GENERAL PAVEMENT MARKING NOTES (SHEETS 09-10):

1. SURFACE TO BE DRY AND CLEAR OF ALL VEGETATION, ROCK, LOOSE FINES, ETC., BEFORE APPLYING MARKINGS.
2. ONE HUNDRED PERCENT OF MATERIAL REMOVED FROM THE SURFACE (PAINT, FOD, SILT, RUBBER, ETC.), INCLUDING MEDIA USED IN THE REMOVAL MUST BE ACCUMULATED, CONTAINED, AND PROPERLY DISPOSED OF OFF AIRPORT PROPERTY.
3. BLACK BORDERS REQUIRED ON ALL RUNWAY MARKINGS.
4. ALL MARKINGS ARE TWO (2) COAT. NO BEADS IN FIRST COAT.



UNDERGROUND SERVICE ALERT
811 DIAL 811
 TWO WORKING DAYS BEFORE YOU DIG

MARKING PLAN

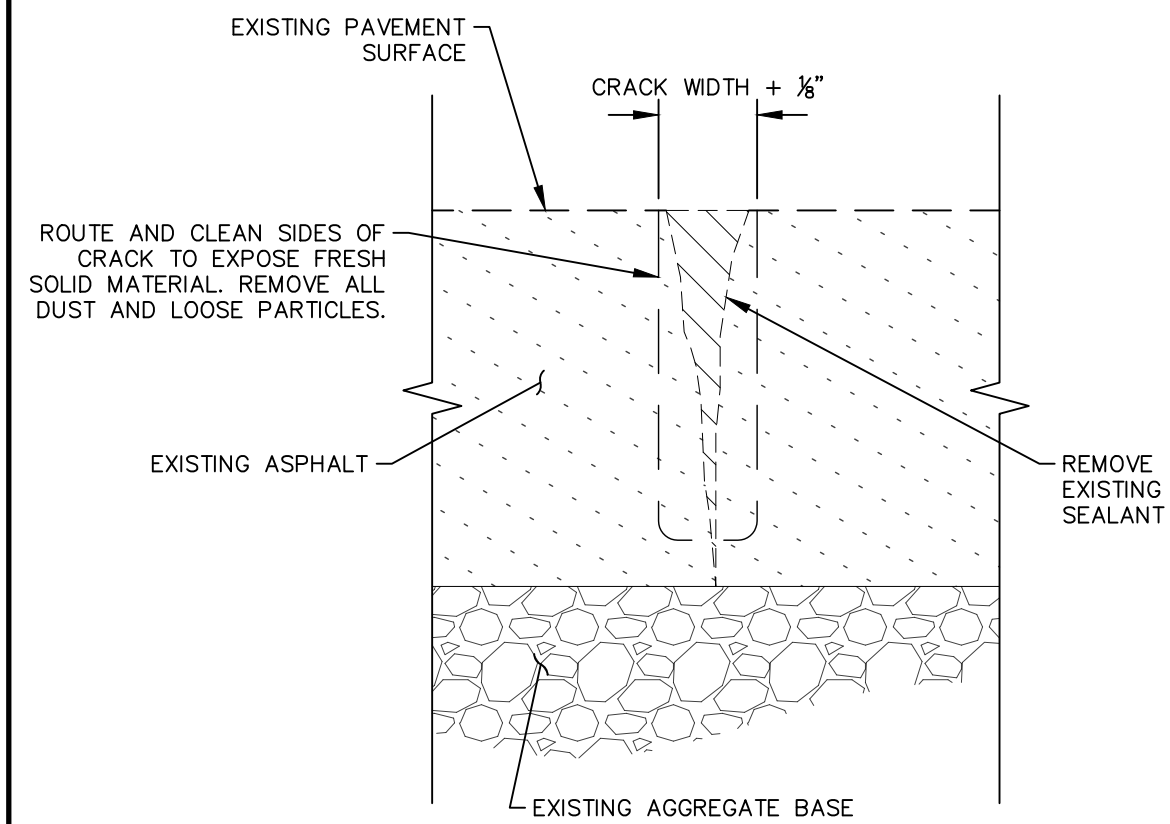
RUNWAY 12-30 STA: 42+00 TO STA. 76+94.81

REV.	DESCRIPTION	DATE	APP.
1			

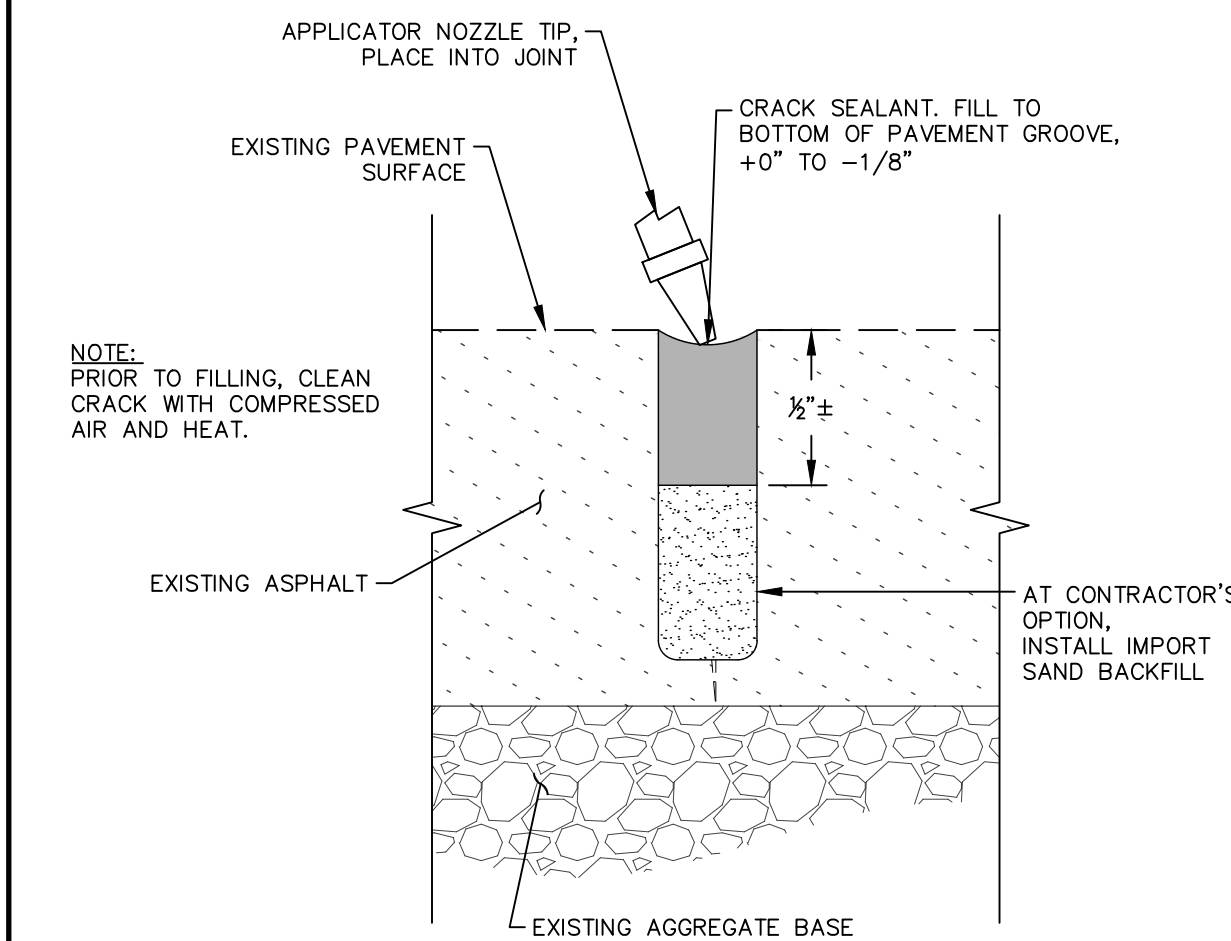
TARTAGLIA ENGINEERING
 BISHOP AIRPORT
 A County of Inyo Aviation Facility
 RUNWAY 12-30 SURFACE TREATMENT

DESIGN JTH
 DRAWN NJG
 CHECKED JAS
 DWG. NO. 24-31
 DATE 06/18/2024
 SHEET 10 of 12

PL01 DATE: 6/20/24



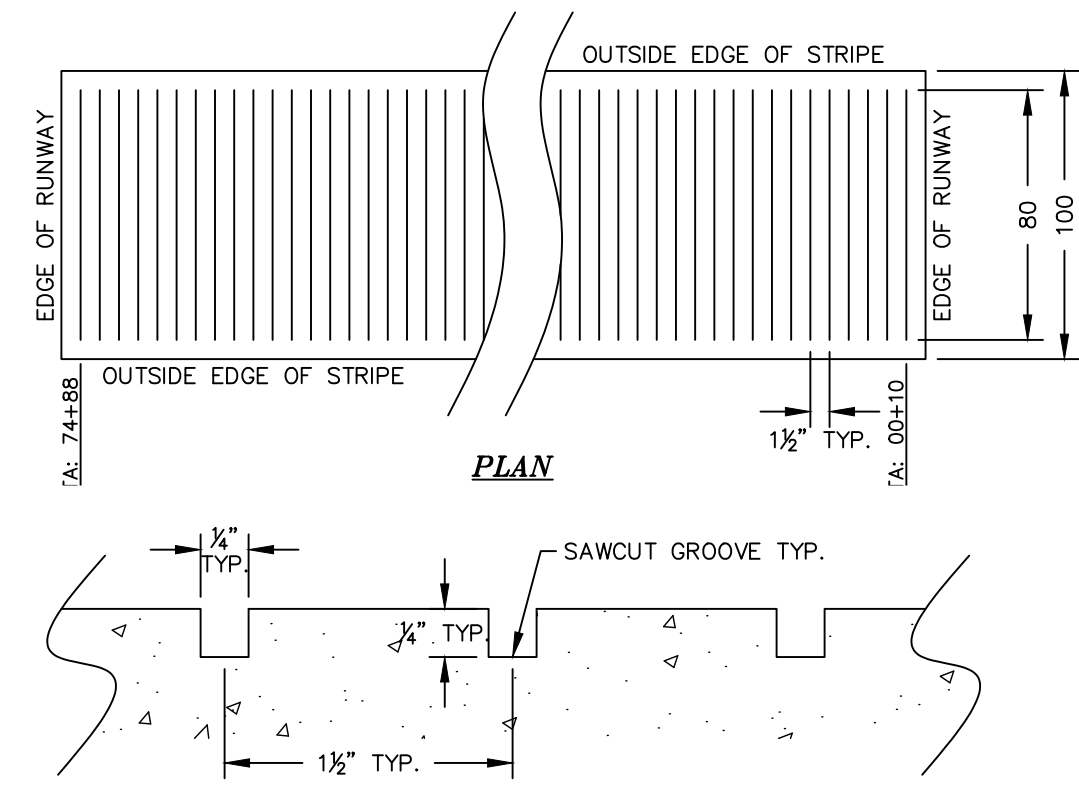
ROUTE SECTION



FILL SECTION

CRACK REPAIR DETAIL

SCALE: 1" = 5'



SECTION

RUNWAY 12-30 GROOVING DETAIL

NO SCALE

UNDERGROUND SERVICE ALERT

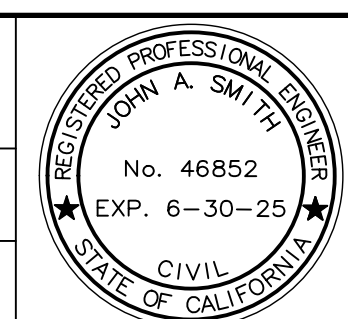


DIAL 811
TWO WORKING DAYS
BEFORE YOU DIG

DETAILS
SURFACE TREATMENT

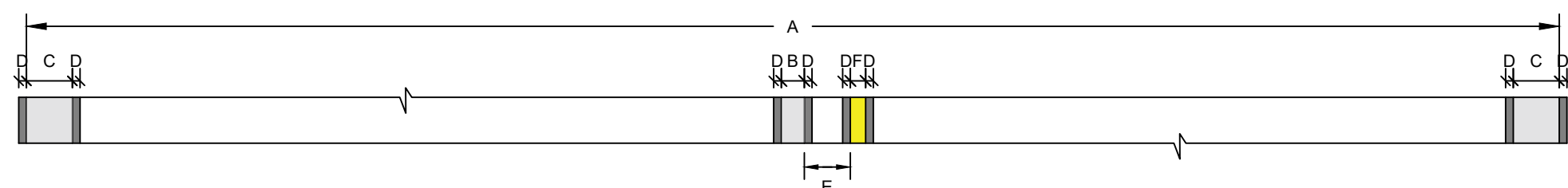
REV.	DESCRIPTION	DATE	APP.
1			
2			
3			
4			
5			

TARTAGLIA ENGINEERING
BISHOP AIRPORT
A County of Inyo Aviation Facility
RUNWAY 12-30 SURFACE TREATMENT



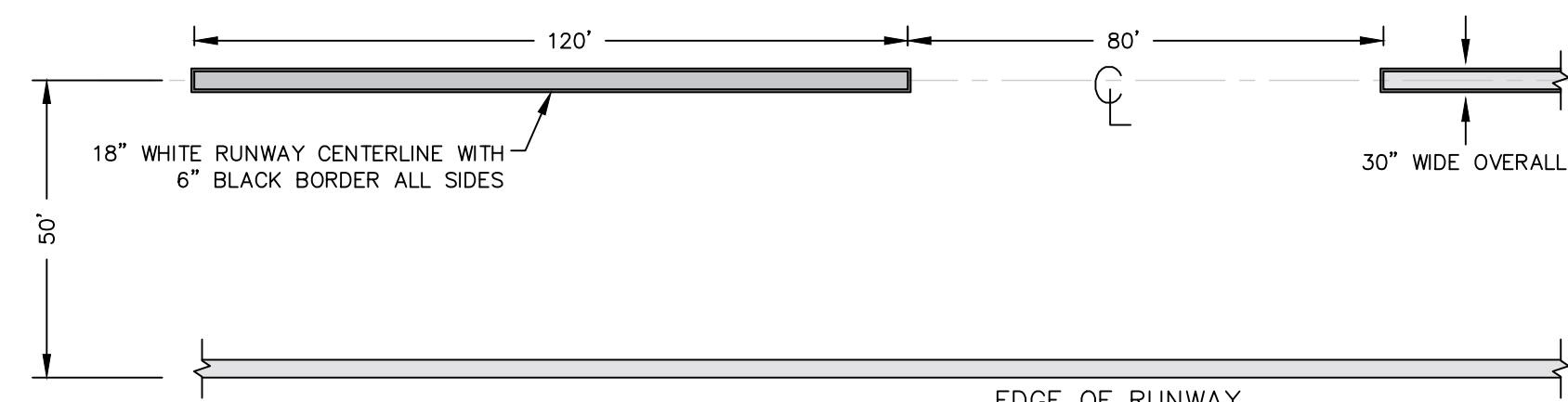
DESIGN JTH
DRAWN NJG
CHECKED JAS
DWG. NO. 24-31
DATE 05/13/2024
SHEET 11 of 12

PL01 DATE: 6/6/2024

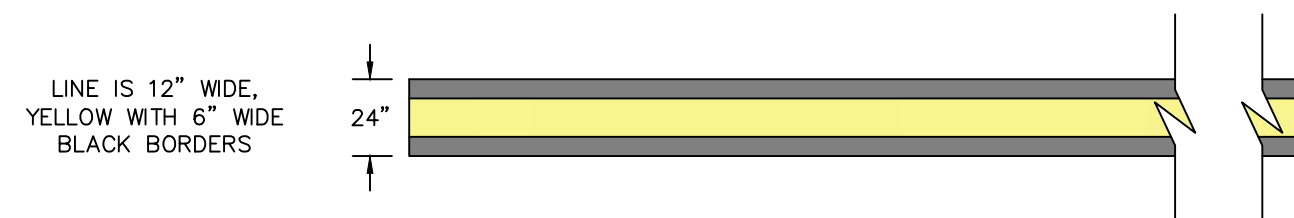


ITEM	DESCRIPTION	COLOR	REFLECTIVE MEDIA	WIDTH
A	RUNWAY WIDTH			100'-0"
B	CENTERLINE MARKING WIDTH	WHITE	YES	1'-6"
C	EDGE MARKING WIDTH	WHITE	YES	3'-0"
D	BORDER WIDTH	BLACK	NO	0'-6"
E	TAXIWAY LEAD-IN-LINE	YELLOW	YES	1'-0"
F	CENTERLINE TO LEAD-IN-LINE SPACING (NEAR EDGE TO NEAR EDGE)			3'-0"

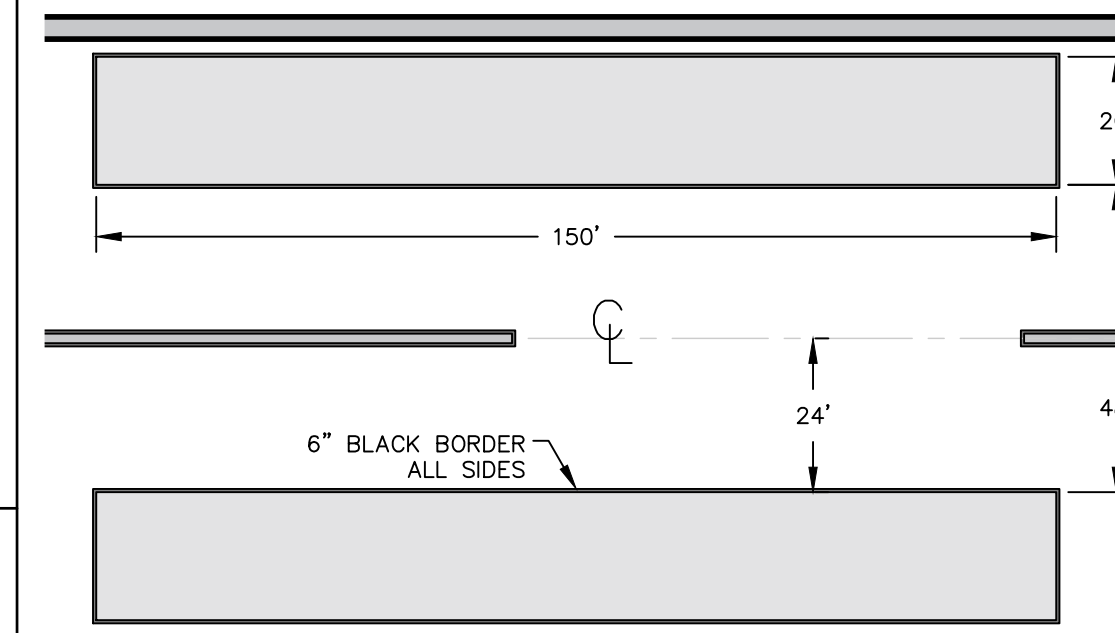
A **12** TYPICAL RUNWAY MARKINGS DETAIL - 100' RUNWAY WIDTH
SCALE: 1" = 10'



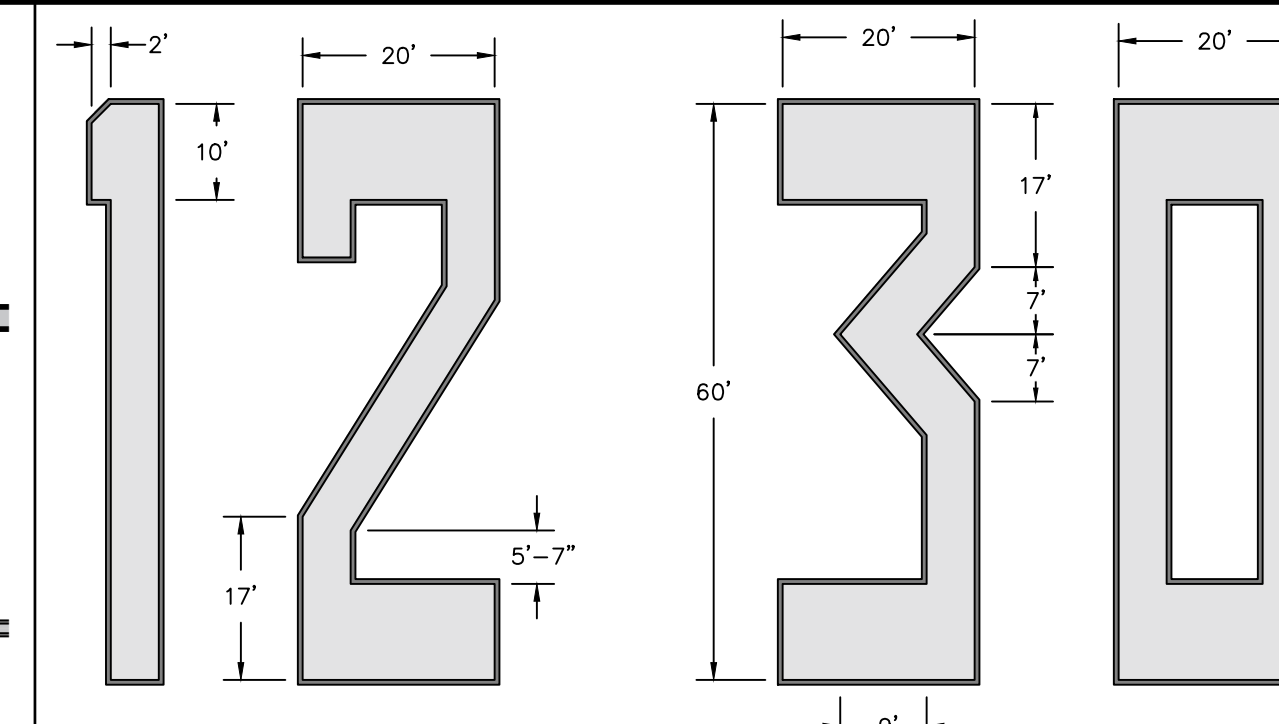
B **12** RUNWAY CENTERLINE & EDGE STRIPE DETAIL
SCALE: 1" = 30'



C **12** TAXIWAY CENTERLINE
SCALE: 1" = 5'

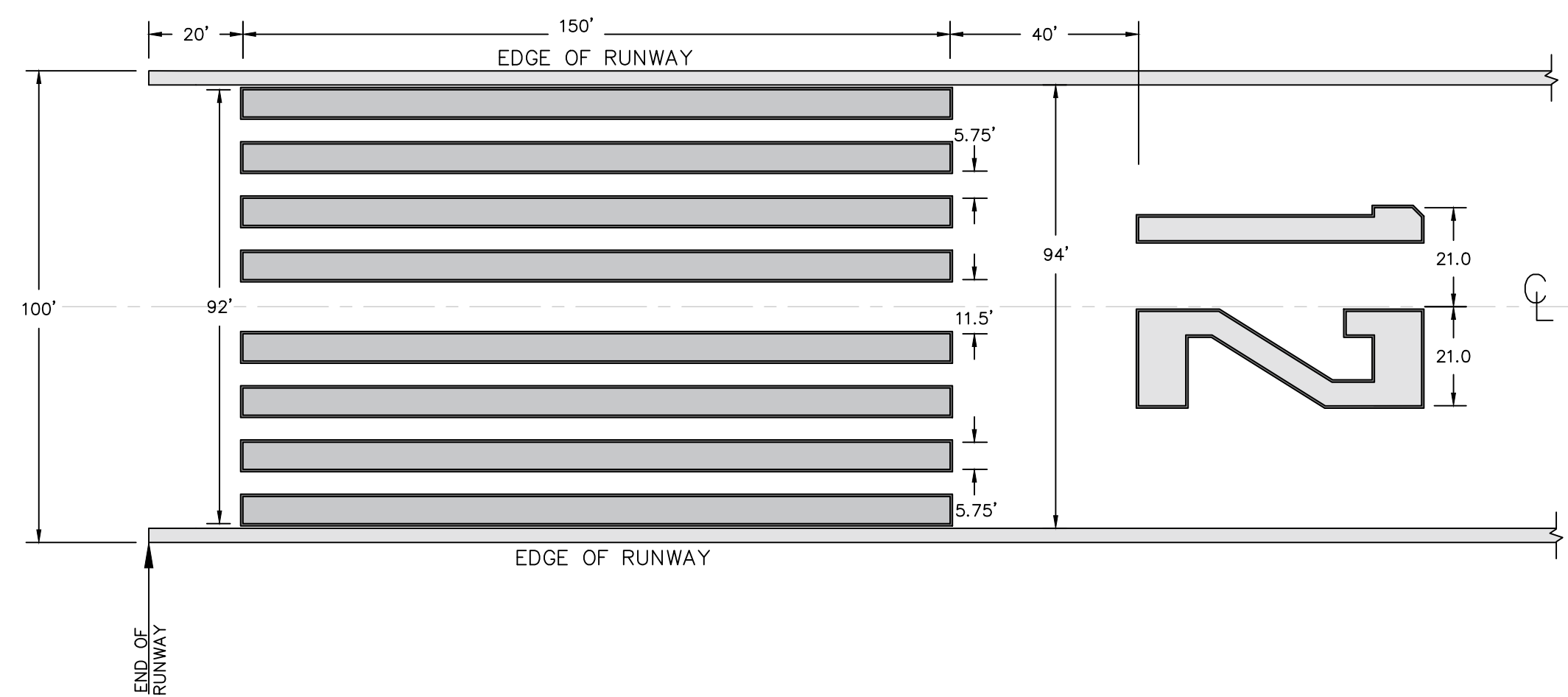


D **12** AIMING POINTS
SCALE: 1" = 30'

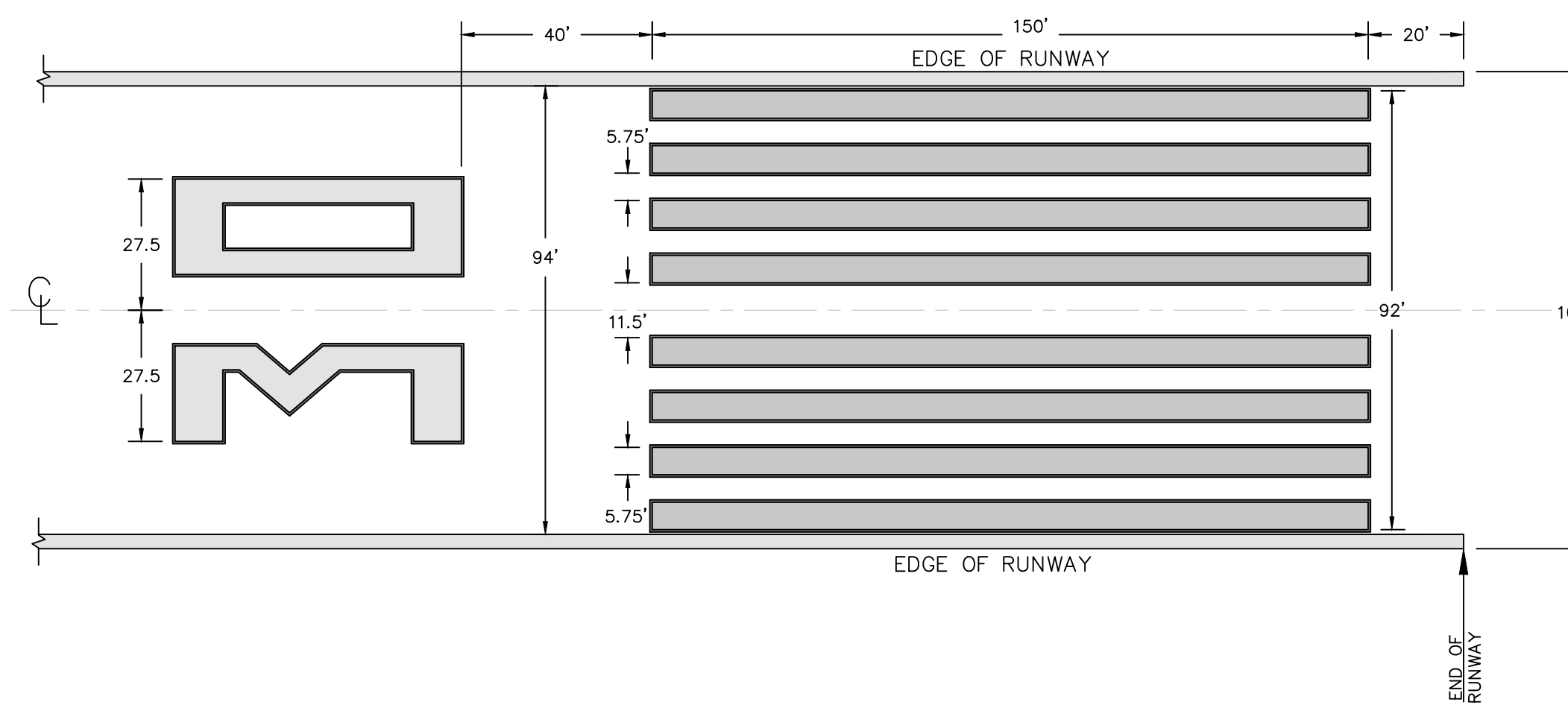


- NOTES:
- DIMENSIONS ARE EXPRESSED IN FEET.
 - ALL CHARACTERS HAVE THESE CHARACTERISTICS (UNLESS OTHERWISE SPECIFIED):
 - 60 HIGH
 - 20 WIDE
 - VERTICAL STROKE OF 5
 - HORIZONTAL STROKE OF 10
 - DIAGONAL STROKE OF 5
 - ALL NUMERALS ARE HORIZONTALLY SPACED 15 APART.
 - SINGLE DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE. FOR DOUBLE DESIGNATIONS, THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS IS CENTERED ON THE RUNWAY PAVEMENT CENTERLINE.
 - ALL CHARACTERS HAVE 6" BLACK BORDERS.

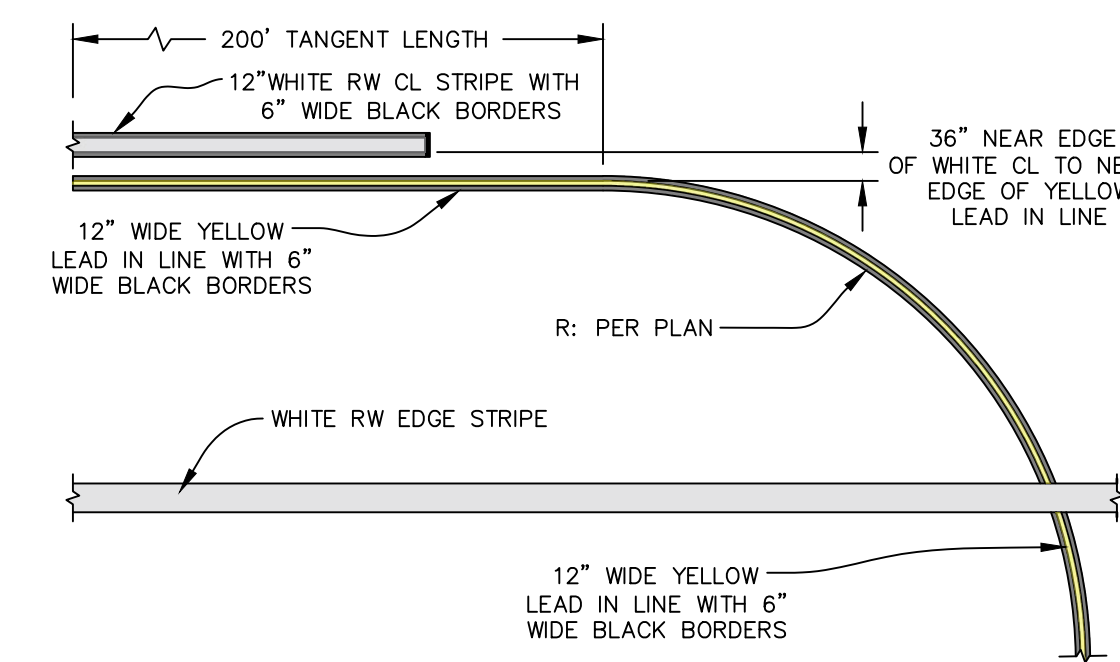
E **12** RUNWAY NUMERALS
SCALE: 1" = 20'



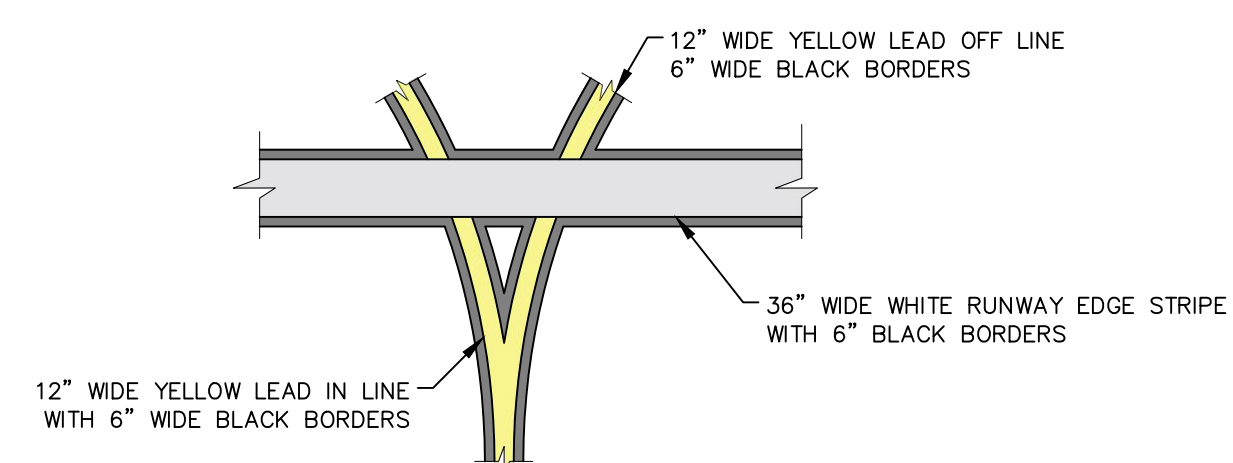
F **12** RUNWAY 12 THRESHOLD MARKINGS
SCALE: 1" = 30'



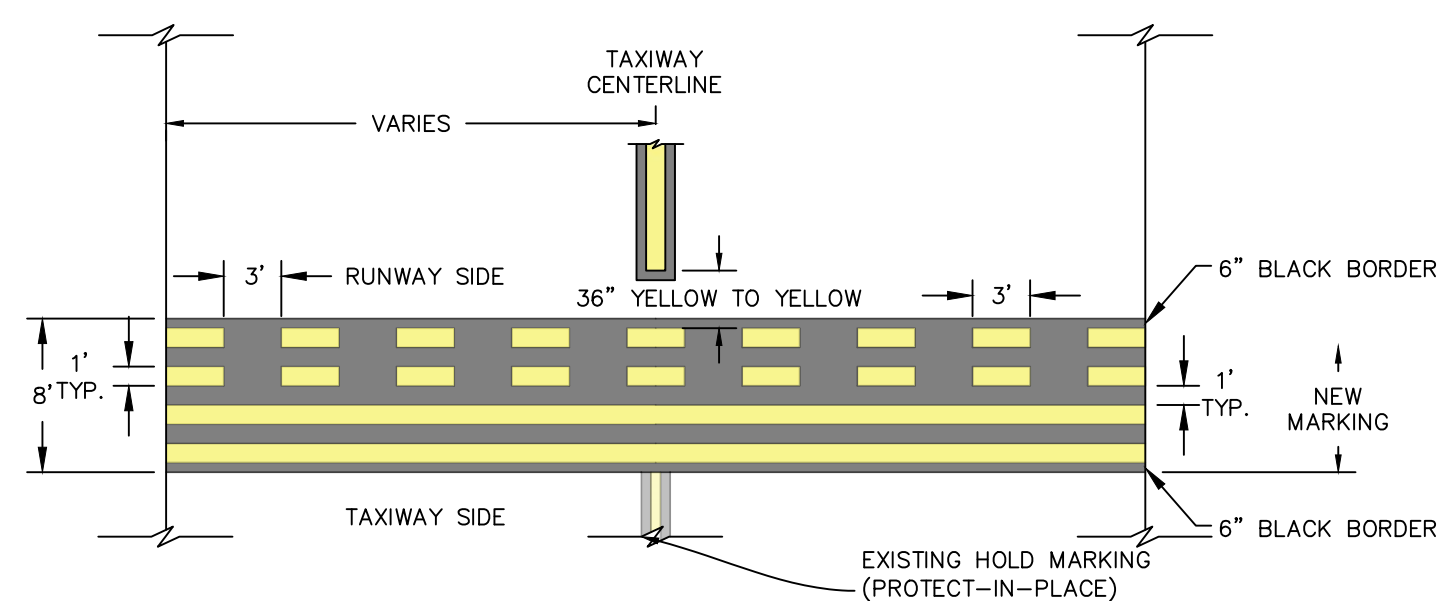
G **12** RUNWAY 30 THRESHOLD MARKINGS
SCALE: 1" = 30'



H **12** TAXIWAY LEAD-OF/OFF LINES
NO SCALE



I **12** STRIPING BREAKS AT RUNWAY INTERSECTION
NO SCALE



J **12** TAXIWAY CENTERLINE AT EXISTING HOLD MARKING
SCALE: 1" = 10'

PERMANENT MARKINGS					
PAINT					GLASS BEADS
AS DISPLAYED PER DETAIL	TYPE	COLOR	FED. STD. 595 NUMBER	APPLICATION RATE MAXIMUM	APPLICATION RATE MINIMUM
II	WHITE	37925	115 FT ² /GAL	III	10 LB/GAL
II	YELLOW	33538 OR 33655	115 FT ² /GAL	III	10 LB/GAL
II	BLACK	37038	115 FT ² /GAL	N/A	NOT USED

K **12** MARKING MATERIALS
NO SCALE

UNDERGROUND SERVICE ALERT
811 DIAL 811
TWO WORKING DAYS BEFORE YOU DIG

DETAILS MARKING

REV.	DESCRIPTION	DATE	APP.
1			

TARTAGLIA ENGINEERING
BISHOP AIRPORT
A County of Inyo Aviation Facility
RUNWAY 12-30 SURFACE TREATMENT

REGISTERED PROFESSIONAL ENGINEER
JACOB A. SMITH
No. 46852
EXP. 6-30-25
CIVIL
STATE OF CALIFORNIA

DESIGN JTH
DRAWN NJG
CHECKED JAS
DWG. NO. 24-31
DATE 05/13/2024
SHEET 12 of 12

PL01.DWG 6/26/2024